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<b>Ref :</b>	<b>205101REM</b>	
<b>Address:</b>	<b>The Straight, Southall Middlesex UB1 1QX</b>	
<b>Ward:</b>	<b>Southall Broadway</b>	
<b>Proposal:</b>	<b>Application for the approval of Reserved Matters pursuant to outline planning permission reference 171562VAR dated 16/10/2017, condition 2 pertaining to the third phase of development (Phase 3): details relating to Layout, Appearance, Scale and Landscaping.</b>	
<b>Drawing numbers:</b>	<b>As set out in Condition 2</b>	
<b>Type of Application:</b>	<b>Reserved Matters</b>	
<b>Application Received:</b>	<b>10/09/2020</b>	<b>Revised:</b>

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**Report by:** Rohan Graham

**Recommendation:** That the Planning Committee GRANT approval of the reserved matters subject to subject to a deed of variation (to secure carbon offset contribution) and conditions contained within Appendix A.

### **EXECUTIVE SUMMARY**

This planning application seeks permission for the approval of reserved matters in respect of the fifth phase of development as required under condition 2 of planning permission reference 171562VAR dated 16<sup>th</sup> October 2017 dealing with the matters of layout, scale, appearance and landscaping. The previous stages have granted planning permission for Phase 1 (residential), Phase 2 (mixed use), the Spine Road and the Linear Park (now the Central Gardens).

Phase 3 seeks to deliver 645 new homes (market and affordable), with associated parking, soft and hard landscaped public spaces and private amenity spaces. It represents the extension to the 'Northern Quarter' residential phase that was partially approved by the Phase 1 development. This phase mirrors the land use and generally street layout that was approved within Phase 1.

The key elements of the scheme as they relate to urban design, residential amenity quality, impact on neighbouring residential occupiers within the recently completed Phase 1 development as well as dwellings along Beaconsfield Road, affordable housing provision, highways, transport, parking, environmental health, Section 106 and Community Infrastructure Levy matters have had their merits assessed in accordance with the development plan policies and any other material planning considerations.

This new development would be laid out in a manner consistent with the approved Outline Masterplan and the adjacent Phase 1 layout. The building scale conforms to the parameters set out in the outline planning permission and drawings provided in the Outline Masterplan application, detailing the maximum and minimum building heights and massing. The buildings would be finished with a variety of brickwork, similar to Phase 1, that would ensure a high-quality of architecture that would enhance the local area. The proposed landscaping for the public realm and private amenity space is considered to be of a high standard and would help establish the sense of place. There have been no objections from third parties.

On balance the proposal would accord with the relevant National, Strategic and Local Planning policies and guidance and approval could reasonably be granted in this case. It is not considered that there are any other material considerations, which would warrant a refusal of the application. It is therefore recommended that Reserved Matters be Approved with Conditions.

### **SUPPORTING DOCUMENTS**

This application was submitted on 30th November 2020 and supported by a full set of Architectural Drawings and other supporting documents including the following:

- Application Form (prepared by Deloitte LLP)
- Covering Letter (prepared by Deloitte LLP)
- Community Infrastructure Levy (CIL) Form (Prepared by Deloitte LLP)
- Planning Statement (Prepared by Deloitte LLP)
- Site Location Plan (Prepared by HTA Design LLP)
- Existing and Proposed Drawings (Prepared by HTA Design LLP)
- Design and Access Statement (Prepared by HTA Design LLP)
- Statement of Community Consultation (Prepared by Berkeley St Joseph)
- Landscape Statement (Prepared by HTA Design LLP)
- Daylight/Sunlight Assessment (Prepared by Hodkinson Consultancy)
- External Lighting Assessment (Prepared by Hodkinson Consultancy)
- Sustainability Assessment (Prepared by Hodkinson Consultancy)
- Energy Statement (Prepared by Hodkinson Consultancy)
- Circular Economy Statement (Prepared by Hodkinson Consultancy)
- Life Cycle Assessment (Prepared by Hodkinson Consultancy)
- Highways and Transport Statement (Prepared by Transport Planning Practice)
- Parking Design and Management Plan (Prepared by Transport Planning Practice)
- Residential Travel Plan (Prepared by Transport Planning Practice)
- Wind Study (Prepared by RWDI)
- Noise Impact Assessment (Prepared by Hann Tucker)
- Fire Report (Prepared by FDS Consult UK)
- Fire Statement (Prepared by FDS Consult UK)

### **RECOMMENDATION**

That the Planning Committee GRANT approval of the reserved matters subject to the following subject to a deed of variation to the S106 and conditions contained within Appendix A.

- Carbon offsetting contribution of £1,177,050;
- Post Construction Energy Monitoring contribution;
  - £7,962 for the automated energy monitoring web-platform and associated officer/consultant time, and
  - £4,824 for the cost of the energy monitoring equipment and data processing (4 years)

### **SITE DESCRIPTION**

The overall development site is approximately 44.7ha in size. The main part of the Site is located within the London Borough of Ealing. The Grand Union Canal (Paddington Branch) however, marks the boundary with the London Borough of Hillingdon (LBH) which encompasses land to the

west of the Main Site that will be required for the construction of access links to the Site (both vehicular and pedestrian). The Western access from the A312 (Pump Lane) has now been completed and all site construction traffic to the site is using the route.

The overall Site comprises a generally triangular parcel of land and is generally level at approximately 32 metres above ordnance datum (AOD). It has largely been remediated over a period of approximately 18 months under close scrutiny and monitoring by the Council and the Environment Agency as regards public health and ground water. There remains several phases of ongoing testing and monitoring.

Phase 1 (formerly Phase A) is partially completed with Blocks A through J completed (although only Blocks A, B, C & D are occupied). Blocks G, H and I remain under construction. Phase 2 (formerly Phase B) has begun construction with a basement currently being excavated. No above ground building works have commenced.

The portion of the wider site that is the subject site for the purposes of this application (Phase 3), is located at the northern end of the site within a precinct identified as 'The Northern Quarter'. It has an area of approximately 1.95 hectares (4.82 acres).

The Phase 3 sites adjoins Phase 1 to the east and residential houses along Beaconsfield Road to the north. The Central Gardens will be located directly to the south of the site. The Phase 3 would be interconnected with the developing road network within the site.

The wider urban area around the Site comprises mainly residential, retail and employment uses. The Site is bound to the south by the London Paddington to Cardiff railway line, to the north by houses fronting Beaconsfield Road (and adjoining roads) to the east by South Road and The Crescent and to the west by the Grand Union Canal and Minet Country Park. The M4 motorway is generally parallel to the railway line to the south with Junction 3 (the A213 intersection), approximately 1.2km to the south west of the Site. At its closest point, the A312 Hayes Bypass is approximately 230m to the west of the Site, beyond the Minet Country Park.

The applicant has rebranded the site from the former Southall Waterside to The Green Quarter. All references in this report to 'The Green Quarter' can be taken to refer to the site formerly known as 'The Southall Gasworks site' and 'Southall Waterside'.

### **RELEVANT PLANNING HISTORY**

The subject site was granted Outline Consent for the Masterplan of the site in September 2010. This Outline Masterplan has been varied on three occasions (not including non-material amendments) since February 2016. There have been four previous phases approved via reserved matters applications.

The planning history is detailed below.

<b>Reference no</b>	<b>Date</b>	<b>Details</b>	<b>Status</b>
195279REM	20 March 2020	Reserved Matters for Linear Park (now referred to as the Central Gardens).	Approved
194098NMA	3 October 2019	Non-material amendment to vary Condition 3 of 171562VAR relating to the approved vertical parameters for Phase 3.	Approved
193322NMA	2 September 2019	Non-material amendment to vary Condition 24 of 171562VAR relating to the delivery sequence for the South Road Bridge widening and improvement works to Merrick Road and the Eastern Access.	Approved
185126FUL	5 July 2019	Construction of an eight storey office	Approved

		building and basement of 7,454sqm (Use Class B1), associated cycle parking and associated works.	
185158REM	22 February 2019	Application for the approval of Reserved Matters pursuant to outline planning permission reference 171562VAR dated 16/10/2017, Condition 2 pertaining to the second phase of development (Phase 2): details relating to Layout, Appearance, Scale and Landscaping.	Approved
185458NMA	10 December 2018	Non-material amendment to vary 171562VAR in relation to the Eastern Gateway.	Approved
178515NMA	23 January 2018	Non-material amendment to vary approved drawings to provide flexibility in the delivery of floor spaces.	Approved
172953REM	27 November 2017	Reserved Matters application for The Spine Road and the Straight.	Approved
171562VAR	16 October 2017	Section 73 application for Minor material amendment to vary condition 3 (approved drawings) and remove condition 41 (details of soakaways).	Approved subject to S.106 agreement
170819VAR	7 September 2017	Section 73 application for Minor material amendment to vary condition 3 to amend the Western Access detailed plans pertaining to the 2016 Masterplan Consent.	Approved subject to S.106 agreement
171094NMA	13 April 2017	Non-material amendment to vary the wording of condition 17 of 2016 Masterplan Consent to be consistent with minor material amendment consent PP/2016/0190.	Approved
160115REM	14 August 2016	Reserved matters application for Phase A	Approved subject to S.106 agreement
PP/2016/0190	9 May 2016	Minor material amendment to vary condition 17 to enable commencement of preliminary works.	Approved subject to S.106 agreement
PP/2015/4682	18 February 2016	<p>Section 73 application for minor amendment to vary condition 3 to amend the outline scheme drawings pertaining to the 2010 masterplan in respect of outline approval for demolition of 22 houses; the remediation of the land and the redevelopment of the site to deliver a large mixed use development including residential, non-food retail, food retail, restaurants, bars and cafes, hotel, conference and banqueting, cinema, health care facilities, education facilities, office/studio units, car park, sports pavilion, an energy centre, multi-storey car park and associated car and cycle parking, landscaping, public realm, open space and children's play space.</p> <p>Full application: new access roads from the Hayes-By-Pass and Southall Town Centre to the application site for vehicle, cycle and pedestrian access, including drainage and a flood relief pond. Widening of South Road across the railway line, for the creation of a bus lane and three new accesses onto Beaconsfield road. Two bridges over the Grand Union Canal and Yeading Brook to provide pedestrian and cycle access to the Minet Country Park and Springfield Road application reference P/2008/3981-S dated 29 September 2010.</p> <p>The quantum of development remains the same as the original scheme with the exception that the proposed development now includes a police facility and public toilets. In summary the proposed</p>	Approved subject to S.106 agreement

		amendments to the scheme are: (1) re-distribution of public realm; (2) re-configuration of the internal street network; (3) re-positioning of the Civic Square; (4) re-locating the primary school; (5) improvements to the canal side; (6) re-configuration of buildings around the retained land; (7) alignment of western entrance on the gasworks site; (8) provision of a commercial access.	
PP/2015/4634	9 February 2016	Non-material amendment to Eastern Access and Beaconsfield Road Access	Approved
PP/2015/1376	21 July 2015	Dismantling of the MAN gas holder and subsequent installation of 2 No. temporary telecommunication masts (30 metres high) and a permanent single telecommunication mast (45 metres high).	Approved. MAN gasholder now demolished
PP/2014/4729	15 July 2015	Details of strategy and method of working for each phase of ground remediation pursuant to condition 36 of planning permission Ref P/2008/3981 dated 29/09/2010.	Approved
P/2011/4649	25 March 2014	Details of site wide construction management plan pursuant to condition 12 of planning permission Ref P/2008/3981 dated 29/09/2010.	Approved
PP/2013/4343	10 December 2013	Non-material amendment to rectify incorrect drawing references to LB Ealing conditions 3, 18, 19, 20, 25, 27 and 28.	Approved
PP/2013/4243	20 November 2013	Certificate of lawful proposed development for the demolition of all existing buildings and structures within the red line application boundary of planning permission ref: P/2008/3981-S as is necessary to implement the development approved by the same planning permission.	Approved
P/2008/3981	29 September 2010	Outline Application for demolition of 22 houses; the remediation of the land and redevelopment of the site to deliver a large mixed use development including residential, non-food retail, food retail, restaurants, bars and cafes, hotel, conference and banqueting, cinema, health care facilities, education facilities, office/studio units, sports pavilion, an energy centre, multi-storey car park and associated car and cycle parking, landscaping, public realm, open space and children's playspace. Full application: New access roads from Hayes by-pass and Southall Town Centre to the application site for vehicle, cycle and pedestrian access, including drainage and a flood relief pond. Widening of South Road across the railway line, for the creation of a bus lane and three new accesses onto Beaconsfield Road. Two bridges over the Grand Union Canal and Yeading Brook to provide pedestrian and cycle access to the Minet Country Park and Springfield Road.	Approved subject to S.106 agreement

## **THE PROPOSAL**

The application is for Reserved Matters as set out under Condition 2 in respect of the third phase of development ("Phase 3") of the redevelopment of the Southall Gasworks site. Condition 2 states: *"No part of the development as hereby permitted shall commence until the reserved matters, as listed below, for that Part have been approved by the London Borough of Ealing as local planning authority:*

- *Layout*
- *Appearance*
- *Scale*
- *Landscaping*

*The relevant Part of the development shall in all respects be carried out in accordance with the approved reserved matters for that Part, unless agreed otherwise in writing with the London Borough of Ealing as the local planning authority.”*

Following the approval of the Phase 1 reserved matters application in August 2016, Phase 2 in February 2019, and the Linear Park (now referred to as the Central Gardens) in March 2020, Phase 3 is the fourth phase of The Green Quarter Masterplan to be brought forward. The consented masterplan proposes a legible structure of open space and public realm defining development plots, seeking to create a series of spatial experiences around which distinctive new neighbourhoods can be delivered. This subject application phase seeks to create a new high-quality residential development. The new residential blocks would be created around new landscaped spaces with pedestrian and cycle routes, which would link the uses to the wider area.

Phase 3 would provide a total of eight blocks ranging in building heights from 3 to 13 storeys and identified as Buildings B to H (inclusively) for this mixed use residential and commercial phase. Buildings E and G include two separate blocks of three (3) townhouses, totalling six (6) townhouse dwellings.

The new homes would range in size from studios up to 3-bedroom apartments and townhouses which would be delivered in a range of tenures in accordance with the approved masterplan and the associated Section 106 agreement.

The buildings would incorporate a materiality that is characteristic of this area with a range of coloured brickwork. Balconies would project off the built form providing private amenity space for every dwelling.

The new buildings would be laid out around a street pattern that is reflective of that developed within Phase 1 and would run parallel and perpendicular to the street network to the north. Parking would be available along the new streets whilst the public domain would be completed with paving and planting.

Having regard to the above, it is reinforced that the purpose of this application is to assess the layout, appearance, scale and landscaping of the proposed built form. The principal of the development and building parameters have been previously established. The planning obligations, such as affordable housing, have been identified within this application, but would be subject to further consideration prior to the occupation of the development and within an ongoing manner across the course of The Green Quarter site.

The proposed residential units and tenure typologies that Phase 3 seeks to deliver are set out below in Table 1 below.

**Table 1: Residential Uses (Tenure type and size)**

Unit Type	Suite	1B2P	2B3P	2B4P	3B5P	Townhouse	Total
<b>Private</b>	43	149	97	147	8	6	<b>450</b>
<b>Shared Ownership</b>	11	27	15	31	0	0	<b>84</b>
<b>Affordable Rent</b>	0	26	19	26	40	0	<b>111</b>
<b>Total</b>	54	202	131	204	48	6	<b>645</b>

Phase 3 would establish an extension to Samara Drive, running east-west through the centre of the site. Additionally, a new north-south street would be established to the west of the Phase 3

buildings. These new streets would provide access to the car, motorcycle and cycle parking that are to be provided across the site.

The car parking would be predominantly located on the ground floor level, beneath a landscaped podium, as well as being located at grade and on-street where appropriate. The main residential car park (beneath the podium) would primarily provide the residential car parking with visitor car parking being provided on street. Two car club spaces are proposed to be provided.

The total car and motorbike parking provision within Phase 3 is as follows.

**Table 2: Car & Motorcycle Parking Provision**

Location	Spaces					
	Regular Car Spaces	Wheelchair Car Parking Space	Visitor Parking	Motorcycle Space	Car Club Parking	Pull-in/service
Ground Floor (Beneath Podium)	27	66	0	41	0	0
Grade	2	13	22	10	2	5
<b>Total</b>	<b>29</b>	<b>79</b>	<b>22</b>	<b>51</b>	<b>2</b>	<b>5</b>

The proposal also seeks to ensure that a cycle friendly environment would be provided with cycle parking spaces being distributed across the site in secure cycle stores for residents and within the public realm for users of the commercial premises. Cycle parking would be provided within 16 separate store rooms that are located across the development.

The following cycle parking spaces are provided as part of the Phase 3 development.

**Table 3: Cycle Parking Spaces**

Building	Spaces
<b>A</b>	54
<b>B</b>	68
<b>C</b>	144
<b>D</b>	148
<b>E</b>	173
<b>F</b>	144
<b>G</b>	173
<b>H</b>	148
<b>A &amp; B External Cycle Storage</b>	90
<b>Total</b>	<b>1142</b>
<b>Visitor Cycle Stands</b>	<b>16</b>

Phase 3 incorporates areas of hard and soft landscaping with paving and tree planting surrounding the new roadways. The roof of the primary residential car park would be covered with a landscaped podium providing a range of hard and soft landscaping as private amenity space for residents. The buildings roofs would be covered with a green roof and Photovoltaic solar panels.

**STATUTORY CONSULTATION**

Neighbour Notification:

The application has been advertised as a major planning application; affecting an area of wider interest. Site notices displayed on 06/01/2021 (expired on 17/02/2021). A Press notice was published on 6 January 2021 (expired on 17 February 2021). Letters sent on 14/12/2020 to Southall Broadway and Southall Green ward councillors.

No written representations have been received.

External

Letters sent on 14/12/2020 (expired on 25/01/2021) to:

- Southall History Society;
- Save Our Southall;
- Health & Safety Executive;
- Civil Aviation Authority;
- Thames Water;
- Canal Rivers Trust;
- London Borough of Hillingdon;
- London Borough of Hounslow;
- Havelock Estate Residents Association;
- Toplocks Residents Association;
- Greater London Authority;
- Transport for London;
- Historic England Archaeology Advisor (GLAAS);
- Historic England;
- Environment Agency;
- National Grid Plant Protection;
- London Fire & Emergency Planning; and
- Metropolitan Police Service.

Four (4) written representations were received. These are outlined below.

**Table 4: External Referral Responses**

Written representation	Case Officers response
<u>Environment Agency</u> No objections.	Noted.
<u>Thames Waters</u> No objections.	Noted.
<u>Historic England</u> No need to consult GLAAS.	Noted.
<u>Transport for London:</u> <ul style="list-style-type: none"> <li>• TfL is pleased that car parking at 0.21 spaces per unit is significantly below the outline consent level (0.7 spaces per residential unit). The implications of this is a lower mode share for cars and increased usage of public transport, in</li> </ul>	Full assessment of the Highways, Transport and Parking impacts is provided below.



<p>keeping with the Mayor’s ambition for 80 per cent of travel in London to be by non-car modes by 2041 (London Plan Policy T1).</p> <ul style="list-style-type: none"> <li>• It is noted that policy-compliant Blue Badge parking will be provided. Active provision of 3% and passive Blue Badge parking provision of an additional 7% will need to be secured by legal agreement and managed, monitored and enforced through a Parking Design and Management Plan (London Plan Policy T4).</li> <li>• The provision of policy-compliant cycle parking (long-stay and short-stay) including provision for wider bicycles is welcomed. There is some concern that cycle parking spaces are too tightly packed and that the LCDS recommended minimum aisle width of 3.5m (2.5m absolute minimum) might not be achieved. However, the access to the cycle stores are acceptable. The provision of 16 visitor spaces are welcome. The location indicated in the landscaped masterplan are suitable.</li> <li>• There are no concerns regarding the street layout and design. The accompanying tracking diagrams demonstrate that the internal roads are suitably designed for the largest vehicles expected to access the site.</li> <li>• The width of footways and the proposed traffic calming features, and tree planting contribute to an environment that aligns with Healthy Streets principles.</li> <li>• Overall, the proposals are acceptable in transport terms, with respect to the access arrangements for all modes, the layout of car parking areas, the layout and design of the internal streets, cycle parking.</li> <li>• TfL is pleased that a reduction in car parking will be net positive in traffic terms, as this results in a lower vehicle trip generation than forecasted.</li> </ul>	
<p><u>London Fire and Emergency Planning:</u> No objection. The applicant is advised:</p> <ul style="list-style-type: none"> <li>• To ensure that the plans conform to Part B of the Approved Document of the Building Regulations and that the application is submitted to Building Control/Approved Inspector who in some</li> </ul>	<p>Noted.</p>

<p>circumstances may be obliged to consult the Fire Authority.</p> <ul style="list-style-type: none"> <li>Guidance note 29 on Fire Brigade Access – Particular attention shown be made to paragraph 16, Water Mains and Hydrants.</li> </ul>	
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Internal Consultees:

Three (3) internal referral responses were received. These are outlined below.

**Table 5: Internal Referral Responses**

Consultee	Comments
Landscape	<ul style="list-style-type: none"> <li>Scale and Massing – The revised layout and massing of the phase 3 blocks creates a more cohesive and interlinked network of communal green spaces running through the site. The communal gardens are now at podium level not ground level which is acceptable and should be providing excellent views out across the Linear Park (Central Gardens).</li> <li>This exciting opportunity has been lost due to the insertion of the six townhouses that block all views of the park from the east and west podium gardens. This visual connection with the linear park (Central Gardens) was very much part of the original concept for all residential phases adjacent to the park and works well in phase 1.</li> <li>Hard and Soft landscaping – the proposed palettes for planting, surfaces, boundary treatment and furniture are all of high quality and the streetscape, podium and garden designs will all provide an attractive range of distinct and welcoming spaces of different characters for both residents and wildlife.</li> <li>Biodiversity – the Urban Greening Factor of 0.32 is acceptable and the use of biodiverse roofs on all blocks including the townhouses is welcomed. The developer is working closely with London Wildlife Trust which is clearly reflected in their approach to planting strategy and mixes and the inclusion of habitat creation opportunities.</li> <li>Play Strategy – this is well thought-out with a good variety of informal play opportunities for younger children integrated into all the communal greenspaces and play space for juniors in the northern garden which will have public access during daylight hours. The child yield for Phase 3 generates a requirement for 2068sqm of dedicated play space of which 1000sqm is being provided on site in the podium and garden play areas. The remaining 1068sqm that covers play provision for older children is being provided outside of this phase in the nearby Linear Park.</li> <li>Amenity Space Provision – the combined areas of the private balconies and terraces, the podium greenspaces and communal gardens are adequate for the 645 units in this phase.</li> <li>It should be noted that the private outdoor amenity space for the six town houses does not meet the minimum 50sqm private garden requirement for houses. In this case the developer may choose to treat them as apartments if they can confirm that the houses will have free access to the communal gardens and podium spaces?</li> <li>In our opinion the following details should be conditioned: <ul style="list-style-type: none"> <li>Details of Hard and Soft Landscaping Scheme</li> </ul> </li> </ul>

	<ul style="list-style-type: none"> <li>○ Details of Boundary Treatment</li> <li>○ Details of raised bed and tree pit soil depths and irrigation</li> <li>○ Details of children’s play area including safety surfacing and play equipment</li> <li>○ Details of a Landscape Management Plan for a minimum period of 5 years.</li> <li>○ Details of the green roof construction and specification, together with a maintenance schedule</li> </ul>
Energy	<p>Despite the concerns about maximising the use of renewable energy, and the long-term heat network strategy (discussed further within the assessment below), the Reserved Matters Application (205101REM) is compliant with the Outline Consent (171562VAR).</p> <p>The overall site-wide CO2 emissions will be cut by at least 47.39%, with 2.68% carbon reduction through “Lean” efficiency measures, 39.87% “Clean” reduction connection to the development site heat network, and 4.84% through “Green” renewable energy PV panels.</p> <p>There is a shortfall of 12,390 tonnes CO2 (over 30 years) in the zero-carbon requirement for the Development and this shortfall will be mitigated through an “offset” S106 payment at £95 per tonne to the Council of £1,177,050.</p>
Housing	<p>This development is being delivered in phases according to the section 106. Phase 3 will include 111 for affordable rent and 84 shared ownership which is a 57% rented and 43% intermediate split. We have met Berkeley homes to discuss the affordable housing provision on this phase and have indicated that our preference is for larger family homes in the rented provision where possible.</p> <p>We would be happy to support the affordable housing mix in this reserved matters application in accordance with that agreed in the section 106 for this large, phased scheme. The rented homes include 40 x 3 bed homes (36%) of the rented tenure which we would strongly support as we have a shortage of larger, family sized accommodation in the borough and this will help towards the provision of homes for those on the councils list for affordable housing.</p> <p>We would be grateful to know which RP partner will be working with Berkeley St Joseph on the this, the latest phase of the development.</p>

**Officers Response:**

Councils Landscape Officer raised a few concerns, whilst broadly supporting the proposal. A key concern was raised regarding the provision of the six terrace houses, their impact on visual connection between the private amenity space and the Central Gardens as well as the requirement for 50sqm of private amenity space.

The provision of the six terrace dwellings has sought to activate the frontage adjacent to the Central Gardens, with the primary view corridor provided through the middle of the site (in between Buildings E & G). The raised podium would not otherwise provide a clear link between the Central Gardens and this site, and the activated frontage is preferred in this circumstance.

Additionally, all of the proposed terrace dwellings would be provided with private amenity space at the rear of the first floor, adjoining the communal amenity space.

**Planning Assessment:**

Development proposals are assessed in terms of their potential impact on the local and wider environment, on the amenities of the occupiers of surrounding development, taking into account the relevant development plan policies for the area and all other material planning considerations.

As a reserved matters application, the main issues for consideration are:

- Urban Design comprising:
  - o Layout;
  - o Scale;
  - o Appearance;
  - o Landscape;
- Impact on amenity of neighbouring properties;
- Standard of Accommodation;
- Affordable housing provision;
- Highways, transport and parking;
- Environmental Health;
- Energy and Sustainability; and
- Section 106 and Community Infrastructure Levy.

**Preamble**

The Outline Masterplan for former Southall Gasworks site was approved in September 2010 (as per planning permission P/2008/3981). An application to vary Condition 3 of Planning Permission P/2008/3981 to revise the approved outline masterplan for the site was submitted to the Council in August 2015 and subsequently granted planning permission subject to a deed of variation of the S.106 in February 2016.

Since February 2016, there have been another three revisions (not including non-material amendments) to the approved site masterplan. These subsequent revisions have retained the key characteristics of the masterplan including the eight (8) 'Character Areas' which are:

- The Eastern Gateway;
- The Emporium;
- Central Park;
- The Northern Quarter;
- Southall Mills;
- Minet Waterside;
- The Canalside; and
- The Western Gateway.

This proposal represents the fifth reserved matters application at The Green Quarter. Previous permission has been granted for Phase 1 (160115REM), Phase 2 (185158REM), the Spine Road (172953REM) and the Linear Park (195279REM), now formerly identified as the Central Gardens.

The approved Phase 1 (formerly Phase A) Reserved Matters application has delivered the first elements of the Northern Quarter which is a residential precinct that seeks to integrate the existing urban fabric with the proposed streets of The Green Quarter. Phase 2 granted permission for 'The Emporium' character area which would establish an activity hub focused around a civic square.

Phase 3 would deliver the second portion of 'the Northern Quarter' with a development that effectively mirrors the bulk and massing of Phase 1. It would provide a built form that is sensitive to the existing terraced houses to the north before gradually increasing in height and scale towards the Central Gardens (formerly identified as the Central Park character area).

The proposal has been assessed having regard to the Outline Masterplan and all relevant planning

policy and guidance. Policies D1, D2, D3, D4, D5, D6, D7, D8, D9, D14, H1, H6, H7, H10, G5, SI1, SI2, T1, T4, T5 and T6.1 of the London Plan (2021), policies 1.1, 1.2, 2.1, 2.8, 5.3, 5.4 and 5.5 of the adopted Ealing Development (or Core) Strategy 2026 DPD; policies 5.10, 5.11, 5.12, 7A, 7.3, 7.4, 7B, 7.7, 7D of the adopted Development Management DPD and Policy SOU5 of the adopted Development Sites DPD and the NPPF are the relevant development plan policies to assess the urban design merits of this reserved matters scheme.

**Urban Design**

***Layout***

Layout Principles

The site layout for Phase 3 has sought to mirror the design and layout of the adjoining residential development in Phase 1. Specifically, the eight buildings would be positioned within a series of landscaped pedestrian areas, with roads and pavements connecting the development within and into the wider surrounding context.

The proposed buildings and street network would be laid out in an orthogonal arrangement which seeks to reinforce the existing urban grain to the north of the site, to match the developing character of Phase 1 as well as to establish a coherent and safe place which fits well within its surrounding.

Phase 3 would not establish any new points of access from outside of the wider site. Rather new vehicular access would be available from the Cedrus Avenue to the east, established within Phase 1, that runs perpendicular from Beaconsfield Road.

Figures 1 illustrates the site location and general layout of the buildings and street network as approved by the Outline Masterplan.



**Figure 1: Location and Layout of Phase 3 within the overall Masterplan**

Building Arrangement

There are a total of eight main buildings proposed on the Site as well as two sets of terraces. Building positioning and orientation are defined by the existing buildings to the north as well as the developing street network.

The building arrangement would remain generally consistent with the pattern established within Phase 1. Six of the buildings would incorporate a frontage to the street network whilst two of the buildings (Buildings E & G) would have a frontage to Greenleaf Walk (to the south) and the private amenity space in the centre of the site.

The buildings have also been arranged by tenure in a similar manner to the Phase 1 development. The affordable rented dwellings would be located at the northern end of the site in Buildings A & B. The shared ownership dwellings would be in Building C whilst also private market dwellings would be in Buildings D – F.

Figure 2 illustrates the building layout approved within the Outline Masterplan whilst Figure 3 shows the proposed building arrangement.



Figure 2: Approved Masterplan Layout for Phases 1 & 3





**Figure 3: Proposed Building Arrangement**

The building arrangement has been slightly altered to deliver the following improvements to the overall layout:

- The building configuration is more ordered creating two C-shapes framing the east and west courtyard which would become comparable in size.
- The central courtyards extend from The Central Gardens to Samara Drive to provide a visual connection from the raised podium to both the street and park.
- The break between Buildings A and B has been increased and altered from a vehicular access to open space.

Ultimately, having specific regard to the proposed landscaping and visual corridors, this is considered to deliver an improved building arrangement and layout for the site.

Open Spaces, Landscaping & Access

Phase 3 does not incorporate any large areas of open space rather it would sit adjacent to the developing Central Gardens located directly to the south. Open space would be provided on the podium and between buildings.

Access into the site would occur off Cedrus Avenue along an extended Samara Drive, which splits Buildings A & B from the rest of the buildings to the south. Samara Drive would be a direct continuation of the central road within Phase 1, thereby allowing for a similarly arranged set of buildings.

### Overall

Overall, the proposed layout is generally consistent with the approved Outline Masterplan whilst allowing for a few improvements to the shape of the buildings to allow for improved visual and landscape connections. As such, the layout is considered to be acceptable.

### Scale

Within the consented masterplan, Phase 3 was originally presented as a mirrored version of the residential Phase 1 (formerly Phase A). This remains the primary intention in terms of the proposed land use and road network. However, through an analysis of opportunities and constraints, Berkeley sought to revise this phase in the following ways:

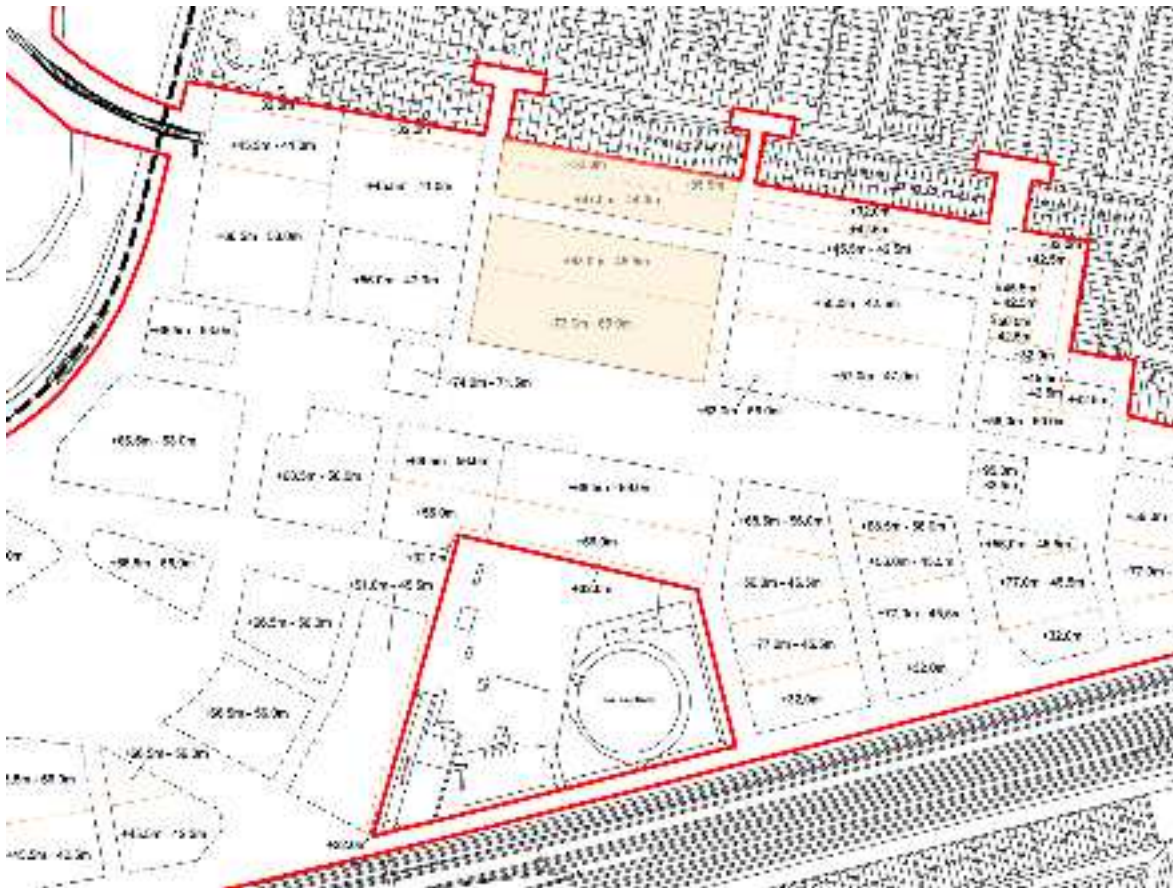
- The proximity of the northern buildings A & B to the existing terrace housing to the north. The proposed protrusions to either side could be minimised to reduce overlooking and overshadowing to the neighbouring properties.
- The proximity between buildings could be reduced to open up larger courtyard spaces and provide more apartments with views to the Central Gardens. This could potentially improve daylight/sunlight ratios throughout the development and less overlooking between apartments.
- Building layouts could take better advantage of the sun path during winter seasons.
- The reorganisation of the public realm and landscaping would strengthen the connection and visual link between the green courtyards and the Central Gardens.

Buildings A and B are 5 storeys (16.2m), Buildings C and F are 6 storeys (20.5m), Buildings D & H are 10 – 11 storeys (35.8m) and Buildings E & G are 13 storeys tall (42.2m). The townhouse buildings would be 3 storeys (11.25m) in height. Figure 4 below illustrates the height for each block and demonstrates how the heights fall down towards the north, where the site adjoins the low-density residential buildings that front Beaconsfield Road.

The maximum and minimum height parameters for buildings are set by the approved Outline Masterplan plot vertical parameter plan approved in 2017. All proposed building structures would sit below the maximum heights established within the parameter plans except for roof plant structures. Nevertheless, these roof plant structures are considered acceptable given:

- They are necessary to the operation of the building;
- They would be setback from all boundaries; and
- They would not be visible when viewed from the ground floor.





**Figure 4: Consented Vertical Parameter Heights**

The scale of the development would be sensitive to and respect the sensitive edges at the northern and eastern boundaries of the site adjoining Beaconsfield Road and Phase 1. The buildings would progressively get larger to the south of the site, with the tallest buildings located in the central part of the site at the southern end. Figures 5 and 6, below, illustrate this height progression across the Phase 3 site.

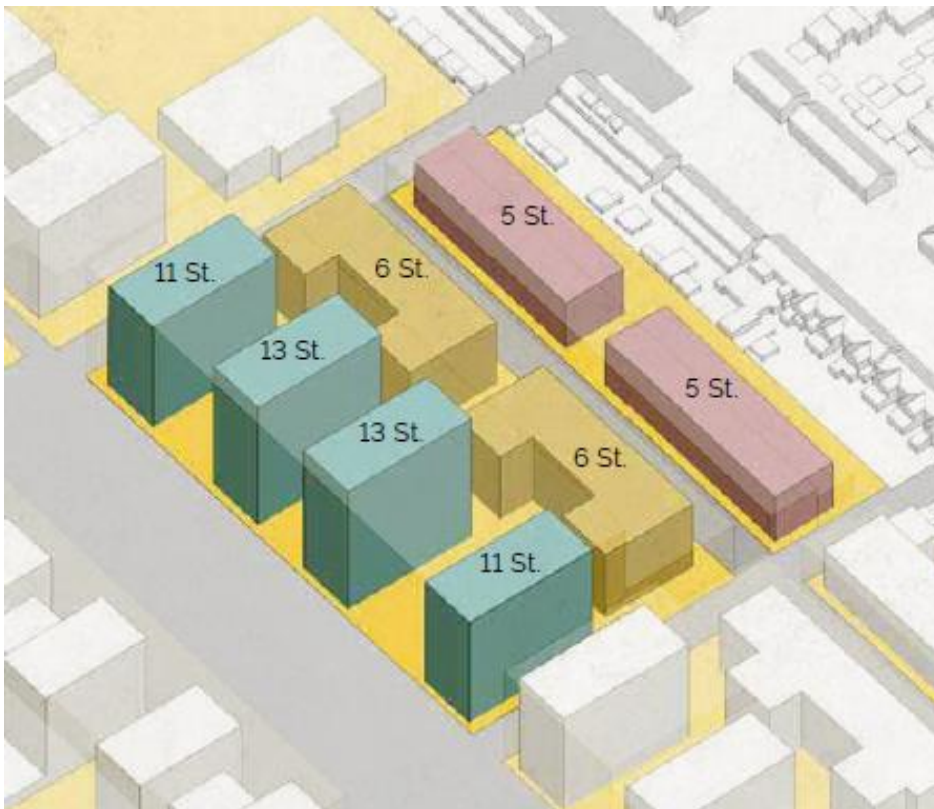


Figure 5: Proposed Building Scale

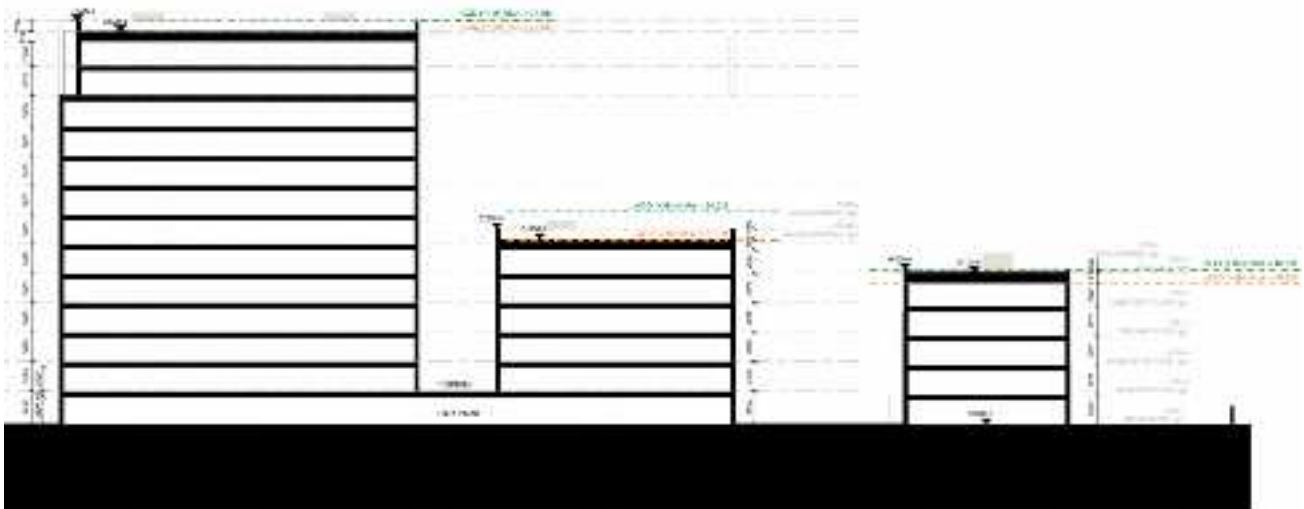


Figure 6: North-East Section (illustrating scale and storeys)

Overall, the scale of the built form has had regard to the context of the site, the Outline Masterplan, and the parameter plans. It would accord with the parameter plans and the current development plan policies and, as such, is considered to be acceptable.

**Appearance**

The appearance of the built form has sought guidance from the established and developing context. Specifically, the nearly complete development at Phase 1 (to the east) has established a precedent that has informed the design and appearance of this proposal.

The strategy for the design of the wider Phase 3 precinct has been defined by an aim to provide a clear hierarchy of streets and buildings which relate in massing, scale and height to the

neighbouring properties. The appearance, materiality and fenestration of these building has been emphasised by the building typology, location and street context.

The built form has been grouped into four groups of architectural propositions being; The Terraces, The Mansion Blocks, The Stepping Streets and The Park View Towers. The appearance of these groups is discussed further below.

Materials

The materiality for Phase 3 has sought to retain the simple yet refined material palette that was incorporated into the Phase 1 development. The buildings would predominantly be constructed and clad in the following materials:

- Brickwork (range of colours);
- Warm grey metal
- Vertically grouped punched windows; and
- Simple bolt-on balconies.

All of the buildings are proposed to share architectural detailing which provides an overall unity to Phase 3 development. The clear differentiation of the buildings' bases using contrasting mortars to distinguish the tripartite elevation. The middle of all façades maintain consistent window proportions and brickwork with the top of the buildings being defined with larger head details, contrasting coloured brick and architectural step backs and pitches.

Condition 6 of 171562VAR as well as a condition of this approval would require the submission of material samples prior to the commencement of works. This ensures that a high standard of materiality would be guaranteed prior to the commencement of works.

Figure 7 illustrates the materiality of the entire Northern Quarter residential phase of development (covered by Phases 1 & 3).

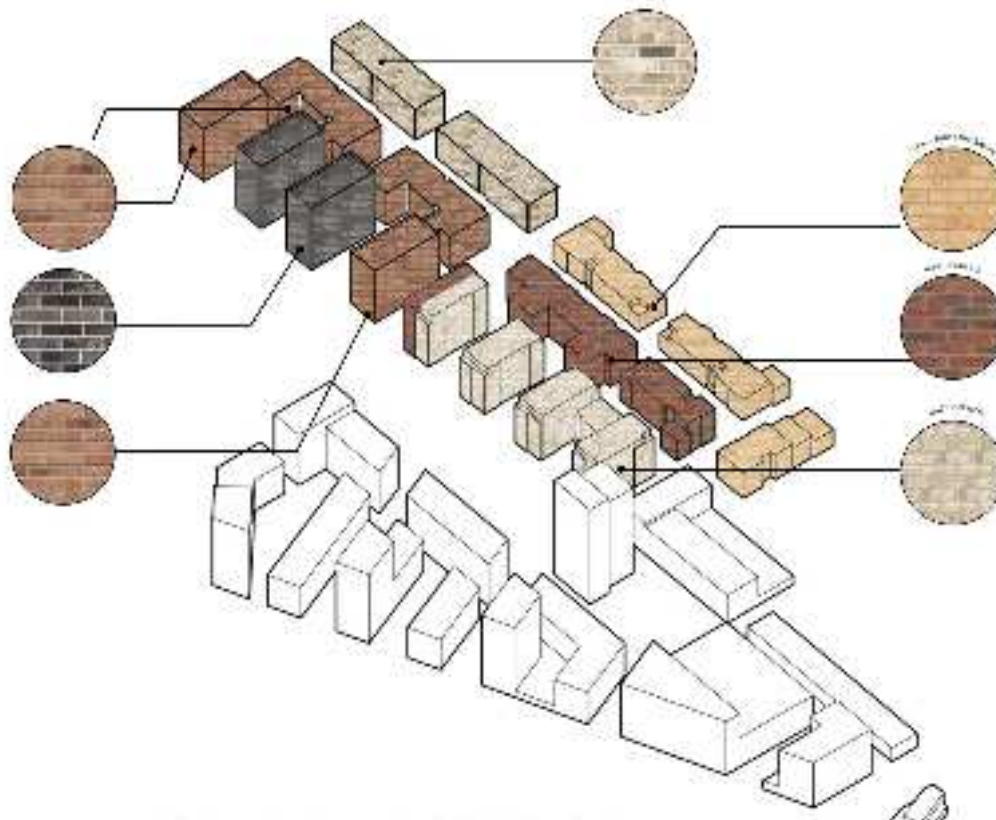


Figure 7: Proposed Materiality of Phase 1 (right) and 3 (left)



Buildings A & B – The Terraces

Buildings A & B are located at the northern end of the site neighbouring the existing Beaconsfield Road terrace houses. The proposal seeks to create a contemporary interpretation of the terraced building typology, reflecting the scale and quality of the existing context. This would allow for a visual connection between the appearance of these sets of buildings whilst allowing for a progression in the height of the built form towards the south.

The elevation of Building A is shown in Figure 8 below.



**Figure 8: Building A Elevation**

Buildings C & F – Mansion Block

The Mansion Blocks would be located to the south of Samara Drive and proposes an intermediate height stepping from the 5 storey terraces up to 6 storeys in line with Phase 1. The Mansion Blocks are a bridge height onto the taller Park View towers which adjoin the Central Gardens to the south. The Buildings have a simple repetitive fenestration which is a contemporary adaptation of the Georgian Mansion Blocks.

Figure 9 illustrates the typical elevation of these buildings (as seen within Building C).



**Figure 9: Building C Elevation**

All Buildings - Stepping Streets

The 'stepping street' typologies construct diverse elevations facing the secondary streets that run north/south - between Phase 1 & 3 and Phase 3/4. The stepping nature of the buildings along these side streets, together with their different facade materialises and fenestration, will add diversity to Phase 3 and will draw people coming from the existing north area of the site, into the Central Gardens.

Buildings D, E, G & H – Park View

The Park View Buildings have the most prominent location within the Phase 3 site and within the wider masterplan site. Buildings E & G are the central point on the Central Gardens between Phases 3 and 4 and as such are proposed as the tallest of the buildings at 13 storeys. These buildings have large balconies that take advantage of the great park views and the wider masterplan.



**Figure 10: Building E Elevations**



**Figure 11: Building H Elevations**

Townhouses

The two separate townhouses buildings (comprising six townhouses) would be three-storeys in height, constructed of buff coloured brickwork and would sit in between two sets of taller buildings. They would have a direct frontage to the Central Gardens.

The appearance and materiality of the townhouse building, within Building E, is shown in Figure 12 below.



**Figure 12: Townhouses (Building E) Elevation**

Podium

A landscaped podium would be established above the ground floor car parking, in between Buildings C, D, E, F & G. The basis for the introduction of a podium is:

- The need to minimise the amount of excavation to limit disturbance to neighbouring properties;
- Reduce the impact on the build programme; and
- Allows for a reduction to the total number of car parking spaces.

Where the podium is visible from street level and the public realm, it would sit above a one-storey brick façade, that would present to the street in a similar manner to the proposed buildings.

Figure 13 illustrates the presentation of the podium above the car park entry.



**Figure 13: Building F Elevation (including car park entry and podium above)**



### Summary

Overall, the proposed buildings appearance would be of a high-quality and standard and would present visually in a manner that is consistent with the existing and developing character of the site.

### ***Landscape***

The Landscape and Public Realm approach for the overall site describes a journey from the vibrant urban streetscape of the town centre and Southall station (east of site) to a soft and naturalistic environment influenced by the character of the Minet Country Park (west of site). This transition is expressed by a proposed green infrastructure and a new pedestrian corridor that flows through the heart of the development, connecting to the Minet Country Park and the adjacent Grand Union Canal.

The landscape masterplan concept is based on a number of themes, which include:

- The flow of water from east to west. Bringing multiple benefits to life across the site.
- Creating a multi-functional Green Infrastructure.
- Establishing an integrated and holistic approach to biodiversity and wildlife through the creation of connective habitats with a significant biodiversity net gain.
- Developing a network of high quality and multi-functional open spaces and unlocking existing open spaces including the Minet Country Park.
- Improvements to health and well-being by creating walkable, playable, inclusive and engaging places for all.
- Increasing connectivity to the canal, adjacent open space, residential areas and the Minet Country Park.
- Ensuring the new community is permeable and accessible to the wider Southall community and natural surroundings.
- Improve access to the Grand Union Canal and Minet Country Park, to help reduce the area of deficiency in access to nature.

Phase 3 seeks the development of a primarily residential development as well as the street network associated specifically with this phase. This site does not include any public open space that will be provided within the wider Green Quarter site.

### Landscape Vision: Phase 3

The stated vision for Phase 3 seeks to establish a high-quality public realm and street network in order to contribute to the creation of a successful public space. The frontage onto the Central Gardens would provide residents within the tallest buildings with a high level of amenity. All residents would be within close proximity to private amenity space and the nearby public open space.

### Landscape Strategy: Phase 3

Within Phase 3, landscaping would be distributed across the scheme, providing amenity and playspace for all residents of the development. The communal amenity space located in between the buildings would be for the use of residents only.

The stated strategy for the public realm is as follows:

- Green the street;
- Maximise green connections;
- Reflect parkland character into courtyards;
- Create planted courtyard gardens;
- Connect the courtyards;
- Integrate play; and
- Bio-diverse planting and habitat types.

To achieve this stated strategy, a range of landscape spaces, corridors, planting and materials are proposed across the scheme. Specifically, the following landscape space would be created:

- Communal Private Open Space (podium level): 3,250sqm
- Defensible planting, building curtilage and private terraces: 2,700sqm
- Public open space: 600sqm
- Public realm, streets, cycle storage and car parks: 4,500sqm

Figure 14 below illustrates the proposed landscape planting.



**Figure 14: Proposed Landscape Planting**

Private Amenity Space & Podium

The private amenity space largely sits in between the proposed buildings and would provide a variety of different spaces for residents to utilise. It is separated into three main podium courtyards and a northern garden (between Buildings A & B).

This space will provide raised decks, children’s play space, social terraces and seating areas that are wrapped in planting and trees.

A view corridor would be established through the site in a north-south orientation. This has been created following the alteration to the building arrangement originally proposed within the Outline Masterplan and allows for an improved landscaping and visual outcome for the site.

Public Domain

The public realm would utilise street and pavement paving as well as trees and vegetation to provide a high-quality public realm that is identifiable as public street space.



The public domain would incorporate the following materials:

- Asphalt road black top;
- Textured concrete flag paving;
- Granite block paving;
- Natural stone slab
- Clay paving; and
- Timber decking.

This hard surface material palette would be accompanied by street trees and planting with a desire for Samara Drive to become 'a leafy tree lined avenue'.

Play Strategy

The Outline Masterplan set out the level of open space to be provided across The Green Quarter, including a requirement for 2.5 hectares play space. The play strategy within Phase 3 is based upon the anticipated child yield for Phase 3.

This strategy balances formal equipped play provision, aimed at older children, set within adjacent parks (which will be provided by different phases) with doorstep play for under-fives in the courtyards, which will be provided as part of this application.

The quantum of playspace to be provided within Phase 3, relative to the Outline Masterplan requirements, are listed within Table 6 below.

**Table 6: Masterplan Site Playspace Quantum**

Age Group	Area (Sqm)	Phase 3 Area (Sqm)
0-5 years (Doorstep Play)	3,900 (0.39 Hectares)	1,000sqm
5-11 years/all ages (Local/Neighbourhood Playable Space)	7,600 (0.76 Hectares)	0
12+ years (Youth Space)	13,500 (1.35 Hectares)	0
<b>TOTAL</b>	<b>25,000 (2.5 Hectares)</b>	<b>1,000sqm</b>

This play space proposal is well thought-out with a good variety of informal play opportunities for younger children integrated into all the communal greenspaces and play space for juniors in the northern garden which will have public access during daylight hours. The child yield for Phase 3 generates a requirement for 2068sqm of dedicated play space of which 1000sqm is being provided on site in the podium and garden play areas. The remaining 1068sqm that covers play provision for older children is being provided by the developer outside of this phase in the nearby Central Gardens and at the existing children's' play area in phase 1.

Green Roofs

Whilst there is no set requirement for green roofs as part of the Outline Masterplan, a green roof would be established above all 8 buildings proposed within Phase 3. The substrate depth of the biodiverse roofs would typically vary between 70-150mm with shallower depts at the edges of the roofs.

Solar Photovoltaic (PVs) panels would be established on the roof of each building above the green roofs.

Overall

The proposed landscaping has been well thought out and would provide for a high Urban Greening Factor despite being located within a part of the site designated for residential development. This would ultimately ensure this area is biodiverse and high in amenity for future residents in line with the vision for the area.

**Impact on amenity of neighbouring properties**

Policies 3.2, 3.5, 3.7, 7.1, 7.3, 7.4, 7.5, 7.6, 7.7 of the London Plan; Policies D3, D4 of the London Plan (2021), Policy SOU5 of the adopted Ealing Development Sites DPD; Policy 7B of the adopted Ealing Development Management DPD and the NPPF are the key development plan policies used to assess the potential impact of the new buildings on the neighbouring properties along Beaconsfield Road as well as the newly constructed Phase 1 dwellings.

The nearest dwellings to the proposed buildings would be Buildings A, F & G within Phase 1 as well as the terrace dwellings to the north fronting Beaconsfield Road.

The distance between the rear of the dwellings on Beaconsfield Road (Nos 205 – 247 Beaconsfield Road) and Buildings A & B would be approximately 25-30 metres. These buildings (A & B) at a height of 5 storeys, would be sufficiently setback from the dwellings on Beaconsfield Road so as to not detrimentally overshadow or impact the daylight and sunlight available to these properties.

A Daylight & Sunlight Report was prepared by Hodkinson to accompany the planning application. This report assessed the daylight and sunlight impacts on the surrounding dwellings as well as the internal daylight levels of the proposed dwellings having regard to the BRE Guidelines 'Site Layout Planning for Daylight and Sunlight – A Guide to Good Practice'. This report provides the following conclusions:

- The initial 25-degree plan test shows the proposal building is unlikely to significantly affect the daylight available to the surrounding properties. Nevertheless, the impact was assessed on 202 windows on surrounding properties. This assessment determined:
  - o 30 windows out of 202 windows assessed on surrounding properties are likely to experience a loss of daylight compared with their existing value.
  - o Only two rooms on the ground floor in two properties slightly fall below the no-sky view BRE targets.
  - o The majority of windows maintain the recommended levels of sunlight during all years and the winter period.
- The proposed scheme does not significantly impact the sunlight levels of the surrounding amenities. All amenities achieved the BRE criteria (100%).
- 90% of all habitable rooms within the proposed development would be adequately lit.

Therefore, the results of this assessment indicate that the proposed development is unlikely to result in a detrimental loss in daylight and sunlight to adjoining dwellings and existing amenity space. Furthermore, the new development has sought to maximise the available daylight and sunlight to the proposed dwellings.

As such, the scheme would have an acceptable impact on neighbouring properties and accord with the relevant development plan policies used to assess this element of the scheme.

**Standard of Accommodation**

Policies D2, D3, D4, D6 and D7 of the London Plan (2021), Policies 3.5 and 7B of the adopted Ealing Development Management DPD; Policy SOU5 of the adopted Ealing Development Sites DPD and the NPPF comprise the relevant development plan policies to assess the quality of the proposed residential accommodation.

**Residential Density**

London Plan (2016) Policy 3.4 'Optimising Housing Potential' (B – appropriate density range) promotes high quality inclusive design that maximizes the potential of sites whilst respecting local context. Policy D2 of the London Plan (2021) states that the density of development proposal should:

- Consider, and be linked to, the provision of future planned levels of infrastructure rather than existing levels; and
- Be proportionate to the site's connectivity and accessibility by walking, cycling, and public transport to jobs and services (including both PTAL and access to local services).

The site has a PTAL of 3 and it would incorporate a density of 339 dwellings per hectare and 860 habitable rooms per hectare. The ongoing density across the site is detailed within Table 7.

**Table 7: Phase 3 Residential Density**

Phase	Setting	PTAL	Standard hr/ha and u/ha for 2.7-3.0 hr/unit	Proposed hr/ha and u/ha
Phase 1	Urban	3	200-450 hr/ha and 70-170 u/ha	167u/ha; 163hr/ha
Phase 2	Urban	3	200-450 hr/ha and 70-170 u/ha	151u/ha; 347hr/ha
Phase 3 (proposed)	Urban	3	200-450 hr/ha and 70-170 u/ha	339u/ha; 860hr/ha

Whilst the density is slightly above the density range recommended by the London Plan (2016), it is generally in accordance with the Outline Masterplan. Both Phases 1 and 2 included larger areas of open space and public domain within their site area which would have otherwise balanced out their density. The density of Phase 3 would eventually be balanced out with the large Central Gardens open space provided directly to the south of the site.

Internal and external amenity space

Policy D6 of the London Plan (2021); and Ealing Local Variation to London Plan policy 3.5 require that housing developments should be of the highest quality internally and externally. Residential proposals should accord with the Table 3.1 of the London Plan (2021) and the Technical Housing Standards - Nationally Described Space Standards 2015 on internal space, layout, and proportion of wheelchair adaptable units.

The internal and external space of all of the proposed dwellings would comply with the relevant standards, as set out within Table 8 below.

**Table 8: Minimum Space Standards**

	Adopted minimum standard internal	Adopted standard private amenity space	Proposed minimum internal	Proposed minimum private amenity space
Studio/Suite	37/39sqm	5 sqm	40sqm	5sqm
1B 2P	50sqm	5 sqm	50sqm	5sqm
2B (3/4)	61/70sqm	6sqm/7sqm	61/70sqm	6/7sqm
3B 5P	86	8sqm	87sqm	8sqm

There will be a variety of dwelling types within Phase 3 ranging from studios/suites up to three-bedroom family homes. All apartment dwellings (except the terraces) will be accessed from a core which provides both lift and stair access to all floors and, where applicable, to the carpark.

The cores will be accessed from the street and also provide access to parking courts or landscaped courtyards. The scheme has full regard to the Mayor’s Housing SPG, complying with the maximum number of dwellings accessed off a core and maximising dual aspect and east-west facing dwellings whilst minimising single aspect north facing homes.

Accessible Dwellings

The dwellings would also comply with lifetimes homes standards and there would be 81 (12.6%) wheelchair units provided within Phase 3. Additionally, level access would be available from the car park to each individual core and lift access, thereby providing accessible access within all buildings.

Overall

On balance, the internal and external design elements of the scheme would accord with the development plan policies and as such would ensure an acceptable level of residential amenity is delivered at this second phase of development.

Affordable Housing provision

Policies GG4, H4, H5, H6 and H7 of the London Plan (2021); Policies 1.2 and 2.8 of the adopted Ealing Development (or Core) Strategy DPD; Policy 3A of the adopted Ealing Development Management DPD; Policy SOU5 of the adopted Ealing Development Sites DPD and the NPPF are the key policies used to assess the affordable housing provision within Phase 3.

The original masterplan scheme ensures that 30% of the residential floorspace across the development would be delivered in the form of affordable housing. The approved Phase 1 has delivered 304 affordable dwellings out of the 623 dwellings. Phase 2, once built, would deliver 146 affordable dwellings out of 751 dwellings.

Phase 3 proposes the delivery of 195 affordable homes comprising 111 affordable rented homes (26 x 1 bed, 45 x 2 beds, and 40 x 3 beds) and 84 shared ownership homes (11 x suites, 27 x 1 bed, and 46 x 2 beds).

There would be 450 open market homes comprising 43 x studios/suites, 160 x 1 bed, 233 x 2 beds, 8 x 3 beds and 6 x townhouses. There would be 30.2% affordable housing provision in terms of dwellings which would increase to 32% when considered in terms of total floorspace.

**Table 9: Unit Type and Tenure**

Unit Type	Suite	1B2P	2B3P	2B4P	3B5P	Townhouse	Total
<b>Private</b>	43	149	97	147	8	6	<b>450</b>
<b>Shared Ownership</b>	11	27	15	31	0	0	<b>84</b>
<b>Affordable Rent</b>	0	26	19	26	40	0	<b>111</b>
<b>Total</b>	54	202	131	204	48	6	<b>645</b>

Figure 15, below, illustrates the affordable housing tenures that have been provided within Phases 1, 2 and now proposed within Phase 3.

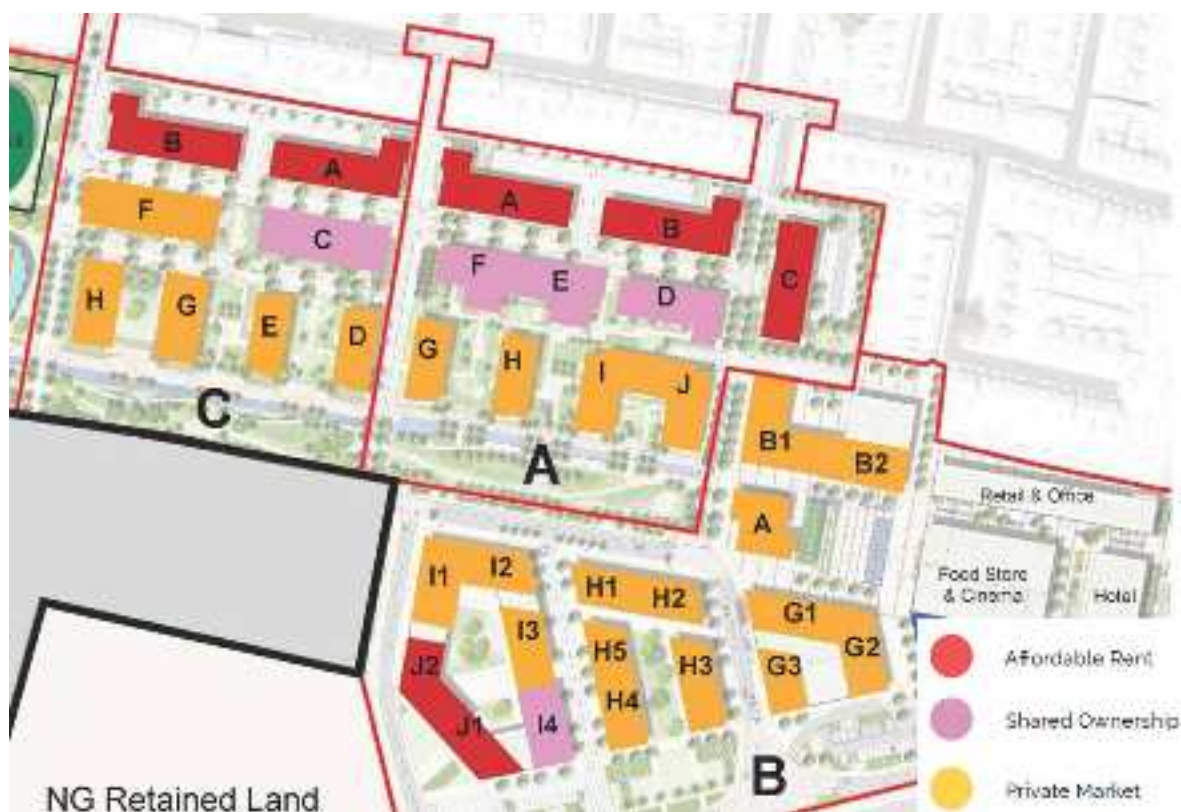


Figure 15: Affordable Housing Provision across Phases 1, 2 and 3

Table 10 below the running total of the housing that has been approved and is being delivered through Phases 1, 2 and that is proposed within Phase 3. It demonstrates that Phase 3 will provide affordable housing in accordance with the 30% floorspace requirement and the wider development is generally tracking in accordance with the requirement under the S106 agreement.

Table 10: Affordable Housing Running Total

Phase	Dwellings total Sqm GEA/Nos	Private dwellings Sqm GEA/ Nos and %	Total Affordable dwellings Sqm GEA/Nos and %	Shared Ownership Dwelling Nos and %	Social rented dwelling Nos and %	Affordable housing split 50/50 Sqm GEA/Nos
<b>Total Dwellings Provision Sqm GEA</b>	320,000	224,000 (70%)	96,000 (30%)	48,000 (50%)	48,000 (50%)	
<b>Total Dwellings Provision Nos.</b>	3,750	2,625 (70%)	1,125 (30%)	563 (50%)	562 (50%)	
<b>Phase A Dwellings Sqm GEA</b>	49,610	23,945 (48%)	25,665 (52%)	13,324 (52%)	12,341 (48%)	52/48
<b>Phase A Dwellings Nos.</b>	623	319 (51%)	304 (49%)	184 (61%)	120 (39%)	61/39
<b>Phase B Dwellings Sqm GEA</b>	88,922	72,465 (81.5%)	16,457 (18.5%)	4,627 (28%)	11,830 (72%)	28/72
<b>Phase B Dwellings Nos</b>	897	751 (83.7%)	146 (16.3%)	40 (27%)	106 (73)	27/73

<b>Phase 3 Dwellings Sqm GEA</b>	41,038	27,938 (68%)	13,100 (32%)	5,050 (38.5%)	8,050 (61.5%)	39/61
<b>Phase 3 Dwellings Nos.</b>	645	450 (69.8%)	195 (30.2%)	84 (43.0%)	111 (57%)	43/57
<b>Balance Remaining Sqm GEA</b>	140,430	99,652 (71%)	40,778 (29%)	24,999 (61.3%)	15,779 (38.7%)	61/39
<b>Balance Remaining Dwelling Nos.</b>	1,585	1,105 (69.7%)	480 (30.3%)	255 (53%)	225 (47%)	53/47

The agreed maximum rental level for the affordable rented accommodation is that it cannot exceed the lower of 80% of market rent or the Local Housing Allowance for each unit size and this includes all eligible service charges.

Following the early delivery of some 48/52 affordable/open market split at Phase 1, the proposal remains generally on track to deliver 30% of the total floorspace as affordable housing.

Under Schedule 1, Part 7.4(a) of the S106 Agreement dated 29th September 2010, the applicant is obligated to tender the affordable housing provision to Registered Providers (RP) prior to the submission of a Reserved Matters Application unless otherwise agreed with the London Borough of Ealing.

Councils Housing Team have indicated that they generally support the affordable housing provision and the proportion of floorspace would be above the 30% required under the Section 106 agreement. As such, the provision and mix of affordable housing is considered to be acceptable.

**Highways, Transport and Parking**

Policies T1, T2, T3, T4, T5, T6, T6.1, T6.5 and T.7 of the London Plan (2021), Policies 1.1 and 1.2 of the adopted Ealing Development (or Core) Strategy DPD; Policy 6.13 of the adopted Ealing Development Management DPD; Policy SOU5 of the adopted Ealing Development Sites DPD and the NPPF are the key policies used to assess the highways, transport and parking provision within Phase 3.

The application was accompanied by the following documentation prepared by Transport Planning Practice (TPP):

- Transport Assessment;
- Residential Travel Plan; and
- Outline Car Parking Management Plan.

**Trip Generation**

A detailed assessment of the impact of trips generated by the wider masterplan has been undertaken as part of the original application as well as the consequent variation application. Given the Phase 3 development forms part of the masterplan site and the quantum of development remains within that approved, the potential impact of the trip generation has been assessed and agreed.

**Car Parking**

Condition 9 of the Outline Consent (171562VAR) sets out the maximum parking provision across the whole site. For residential parking a maximum of 0.7 spaces per unit across the site is permitted.

A total of 133 residential car parking spaces are proposed for Phase 3 which equates to a parking ratio of 0.21 spaces per unit. Of these, 109 spaces will be allocated to residents and 22 for visitors.

In addition, 2 car club spaces are proposed. The bulk of these spaces are located on the ground floor beneath the podium.

A total of 53 Motor Cycle Parking spaces are proposed, including 41 beneath the podium, and these would typically be counted towards the residential parking total. Nevertheless, the proposal remains well within the 0.7 space maximum allowed.

A swept path analysis was carried out to demonstrate that the vehicular access into the car park is acceptable.

A Car Parking Management Plan was prepared by Transport Planning Practice (TPP) and accompanied the planning application. This plan appropriately sets out how the parking provision would be adequately managed, maintained and controlled whilst also minimising any impact on the road network.

Residents of the proposed development would not be eligible for on-street car parking permits.

Transport for London were supportive that the car parking provision was significantly below the consented maximum level (0.7 spaces per residential unit). This is in line with Policy T1 of the London Plan (2021) and, as such, the parking provision is considered to be acceptable.

#### Electric Vehicle Charging Points

Electric vehicles charging points would be provided in line with the requirements of the London Plan (2021). This requires 20% of bays to have charging facilities with the remaining 80% to incorporate passive infrastructure to allow the future inclusion of charging facilities when required.

#### Cycle Parking

A total of 1,142 residential cycle spaces as well as 16 visitor cycle spaces are proposed within 16 separate storage spaces across the site. Each building includes at least one store room to ensure equitable access to this cycle storage.

Through the provision of 1142 residential cycle spaces, the proposal would be compliant with the cycle parking requirements of the London Plan (2021).

#### Overall

The proposed street layout is generally consistent with the broad parameters of the consented masterplan scheme approved in October 2017. The parking provision has been minimised in line with the strategic direction of the London Plan (2021) whilst suitable electric vehicle charging points and cycle parking has been provided throughout the scheme. Therefore, the transport, highway and parking elements of the scheme have been assessed and are considered to generally accord with the relevant development plan policies and as such are acceptable.

#### Environmental Health

Key development plan policies used to assess the environmental impact of the scheme would include Policies S11, S17, S112, S113 of the London Plan (2021), Policies 1.1, 2.1 and 2.8 of the adopted Ealing Development (or Core) Strategy DPD; Policies 5.21 and 7A of the adopted Ealing Development Management DPD; Policy SOU5 of the adopted Ealing Development Sites DPD and the NPPF.

The outline planning permission includes the relevant conditions to protect the environment and the amenities of existing and future residents with regards to remediation, noise, vibration and air quality. Therefore, the application would accord with the above policies and is considered to be acceptable as regards environmental health matters.

### **Energy & Sustainability**

Policies G1, SI1, SI2, and SI3 of the London Plan (2021), Policy 5.2 of the adopted Ealing Development Management DPD; Policy SOU5 of the adopted Ealing Development Sites DPD and the NPPF are the key policies to assess the energy and sustainability requirements for the development of this scheme.

The proposal is required to comply with Condition 49 of 171562VAR which requires the submission of a Compliance Statement outlining the measures to be incorporated into the development to comply with the relevant Building Regulations and London Plan.

The applicant prepared a Sustainability Statement (including Circular Economy Statement) and an Energy Statement and Whole Life Cycle Carbon Emissions Statement both prepared by Hoskinson. The proposal has incorporated a range of sustainability features including:

- All residential units would be built to achieve Code Level 4 rating;
- The development would achieve a minimum 35% reduction in regulated CO2 emissions;
- All dwellings would target a maximum internal daily water consumption of 105 litres/person/day;
- The site incorporates sustainable urban drainage systems;
- The site would benefit from a good existing public transport coverage and sustainable transport modes will be encouraged; and
- Enhancements will be implemented through the provision of open private amenity space, biodiverse roofs and surrounding landscaping.

Councils Energy Consultant raised concern that the proposal was not been assessed against SAP10 but against the old SAP2012 benchmark, and as such is not entirely in-line with current London Plan policy interpretation. Although SAP10 is not yet fully in use, GLA Guidance has encouraged the use of SAP10 carbon emission factors rather than the former SAP2012 emissions factors.

The Strategy is assessed against SAP2012, because Phase 3 will connect to the existing/planned "local" heat network for phases 1&2. Whilst this approach was questioned by Councils Energy consultant, the reserved matters proposal was deemed to be compliant with the Outline Consent (171562VAR).

Overall, the proposal has incorporated sustainable design and construction methods to meet the relevant regulations and conditions set out by the Outline Consent (171562VAR).

### **Section 106 Planning Obligations**

The application proposal does not provide any additional development that requires mitigation outside of the clauses of the extant Section 106 planning obligations and as such, no additional clauses are recommended by way of a deed of variation or separate unilateral undertaking.

### **Community Infrastructure Levy**

The Mayor's Community Infrastructure Levy (CIL) was adopted on 01/04/2012 post the 2010 planning permission. This has introduced a charging system within Ealing of £35 per sqm of gross internal area (GIA) to be paid to the GLA. The application does not increase the amount of development and as such there remains no requirement to pay CIL.

### **Conclusion**

This planning application seeks permission for the approval of reserved matters in respect of the Phase 3 residential development within The Green Quarter at Southall as required under condition



2 of planning permission reference 171562VAR dated 16th October 2017. The reserved matters relate specifically to the layout, scale, appearance and landscaping of the residential development.

Phase 3 seeks to deliver 645 new homes (market and affordable) with associated amenity space, public realm, car, motorcycle and cycle parking. This new development would be laid out in a manner consistent with the approved Outline Masterplan and the adjacent Phase 1 layout. The building scale conforms to the parameters set out in the outline planning permission and drawings provided in the Outline Masterplan application, detailing the maximum and minimum building heights and massing.

The buildings would be finished with a variety of brickwork, similar to Phase 1, that would ensure a high-quality of architecture that would enhance the local area. The proposed landscaping for the public realm and private amenity space is considered to be of a high standard and would help establish the sense of place.

As such, the key elements of the scheme as they relate to urban design, standard of residential accommodation, impact on neighbouring residential occupiers, affordable housing provision, highways, transport and parking, environmental health, energy and sustainability, Section 106 and Community Infrastructure Levy matters have had their merits assessed in accordance with the development plan policies, the Outline Masterplan and all other material planning considerations. There have been no objections from third parties.

On balance the proposal would accord with the relevant National, Strategic and Local Planning policies and guidance and approval could reasonably be granted in this case. It is not considered that there are any other material considerations, which would warrant a refusal of the application.

It is therefore recommended that planning permission be **Approved for the reserved matters application for Phase 3 subject to conditions.**

### **HUMAN RIGHTS ACT:**

In making your decision, you should be aware of and take into account any implications that may arise from the Human Rights Act 1998. Under the Act, it is unlawful for a public authority such as the London Borough of Ealing to act in a manner, which is incompatible with the European Convention on Human Rights.

You are referred specifically to Article 8 (right to respect for private and family life), Article 1 of the First Protocol (protection of property). It is not considered that the recommendation for approval of the grant of permission in this case interferes with local residents' right to respect for their private and family life, home and correspondence, except insofar as it is necessary to protect the rights and freedoms of others (in this case, the rights of the applicant). The Council is also permitted to control the use of property in accordance with the general interest and the recommendation for approval is considered to be a proportionate response to the submitted application based on the considerations set out in this report.

### **APPENDIX A CONDITIONS AND INFORMATIVES**

#### **CONDITIONS:**

1. The development permitted shall be begun before the expiration of three years from the date of this permission.

Reason: In order to comply with the provisions of the Town and Country Planning Act 1990 (as amended).

Approved plans

2. The development hereby permitted shall be carried out in accordance with the following approved plans

A726-BSO-A-SCH-00001 (Revision P2; dated 15/12/2020); A726-HTA-A-REP-00015 (Revision P2; dated 15/12/2020; partially revised on 29/01/2021); A749-HTA-00-A-DR-12100 (Revision P9; dated 29/01/2021); A749-HTA-03-A-DR-12103 (Revision P8; dated 29/01/2021); A749-HTA-RF-A-DR-12107 (Revision P8; dated 29/01/2021); A750-HTA-00-A-DR-12200 (Revision P7; dated 29/01/2021); A750-HTA-03-A-DR-12203 (Revision P7; dated 29/01/2021); A750-HTA-RF-A-DR-12207 (Revision P8; dated 29/01/2021); A751-HTA-00-A-DR-12300 (Revision P10; dated 29/01/2021); A751-HTA-01-A-DR-12301 (Revision P11; dated 29/01/2021); A751-HTA-02-A-DR-12302 (Revision P7; dated 29/01/2021); A751-HTA-04-A-DR-12304 (Revision P11; dated 29/01/2021); A751-HTA-RF-A-DR-12307 (Revision P9; dated 29/01/2021); A752-HTA-00-A-DR-12400 (Revision P6; dated 29/01/2021); A752-HTA-01-A-DR-12401 (Revision P7; dated 29/01/2021); A752-HTA-02-A-DR-12402 (Revision P3; dated 29/01/2021); A752-HTA-03-A-DR-12403 (Revision P5; dated 29/01/2021); A752-HTA-04-A-DR-12404 (Revision P6; dated 29/01/2021); A752-HTA-RF-A-DR-12411 (Revision P5; dated 29/01/2021); A753-HTA-00-A-DR-12500 (Revision P5; dated 29/01/2021); A753-HTA-01-A-DR-12501 (Revision P5; dated 29/01/2021); A753-HTA-02-A-DR-12502 (Revision P5; dated 29/01/2021); A753-HTA-04-A-DR-12504 (Revision P4; dated 29/01/2021); A753-HTA-11-A-DR-12511 (Revision P6; dated 29/01/2021); A753-HTA-RF-A-DR-12512 (Revision P4; dated 29/01/2021); A754-HTA-00-A-DR-12600 (Revision P7; dated 29/01/2021); A754-HTA-01-A-DR-12601 (Revision P7; dated 29/01/2021); A754-HTA-02-A-DR-12602 (Revision P4; dated 29/01/2021); A754-HTA-04-A-DR-12604 (Revision P4; dated 29/01/2021); A754-HTA-RF-A-DR-12606 (Revision P5; dated 29/01/2021); A755-HTA-00-A-DR-12700 (Revision P8; dated 29/01/2021); A755-HTA-01-A-DR-12701 (Revision P8; dated 29/01/2021); A755-HTA-02-A-DR-12702 (Revision P9; dated 29/01/2021); A755-HTA-04-A-DR-12704 (Revision P8; dated 29/01/2021); A755-HTA-11-A-DR-12711 (Revision P8; dated 29/01/2021); A755-HTA-RF-A-DR-12712 (Revision P7; dated 29/01/2021); A756-HTA-00-A-DR-12800 (Revision P9; dated 29/01/2021); A756-HTA-01-A-DR-12801 (Revision P9; dated 29/01/2021); A756-HTA-02-A-DR-12802 (Revision P5; dated 29/01/2021); A756-HTA-03-A-DR-12803 (Revision P5; dated 29/01/2021); A756-HTA-04-A-DR-12804 (Revision P9; dated 29/01/2021); A756-HTA-RF-A-DR-12813 (Revision P6; dated 29/01/2021); A755-HTA-00-A-DR-12513 (Revision P3; dated 29/01/2021); A755-HTA-01-A-DR-12514 (Revision P3; dated 29/01/2021); A755-HTA-02-A-DR-12515 (Revision P3; dated 29/01/2021); A755-HTA-RF-A-DR-12516 (Revision; P3; dated 29/01/2021); A755-HTA-00-A-DR-12713 (Revision P7; dated 29/01/2021); A755-HTA-01-A-DR-12714 (Revision P7; dated 29/01/2021); A755-HTA-02-A-DR-12715 (Revision P7; dated 29/01/2021); A755-HTA-RF-A-DR-12716 (Revision P3; dated 29/01/2021); A726-HTA-00-A-DR-12001 (Revision P21; dated 29/01/2021); A726-HTA-01-A-DR-12002 (Revision P10; dated 29/01/2021); A726-HTA-02-A-DR-12003 (Revision P5; dated 29/01/2021); A726-HTA-03-A-DR-12004 (Revision P10; dated 29/01/2021); A726-HTA-04-A-DR-12005 (Revision P3; dated 29/01/2021); A726-HTA-05-A-DR-12006 (Revision P4; dated 29/01/2021); A726-HTA-06-A-DR-12007 (Revision P4; dated 29/01/2021); A726-HTA-07-A-DR-12008 (Revision P4; dated 29/01/2021); A726-HTA-08-A-DR-12009 (Revision P8; dated 29/01/2021); A726-HTA-09-A-DR-12010 (Revision P4; dated 29/01/2021); A726-HTA-10-A-DR-12011 (Revision P8; dated 29/01/2021); A726-HTA-11-A-DR-12012 (Revision P7; dated 29/01/2021); A726-HTA-12-A-DR-12014 (Revision P3; dated 29/01/2021); A726-HTA-RF-A-DR-12013 (Revision P8; dated 29/01/2021); A749-HTA-EV-A-DR-21110 (Revision P6; dated 20/11/2020); A749-HTA-EV-A-DR-21111 (Revision P6; dated 20/11/2020); A750-HTA-EV-A-DR-21210 (Revision P8; dated 20/11/2020); A750-HTA-EV-A-DR-21211 (Revision P8; dated 20/11/2020); A751-HTA-EV-A-DR-21310 (Revision P9; dated 20/11/2020); A751-HTA-EV-A-DR-21311 (Revision P9; dated 20/11/2020); A752-HTA-EV-A-DR-21410 (Revision P4; dated 20/11/2020); A752-HTA-EV-A-DR-21411 (Revision P4; dated 20/11/2020); A753-HTA-EV-A-DR-21510 (Revision P6; dated 20/11/2020); A753-HTA-EV-A-DR-21511 (Revision P6; dated 20/11/2020); A753-HTA-EV-A-DR-21512 (Revision

P3; dated 20/11/2020); A753-HTA-EV-A-DR-21513 (Revision P3; dated 20/11/2020); A754-HTA-EV-A-DR-21610 (Revision P9; dated 20/11/2020); A754-HTA-EV-A-DR-21611 (Revision P9; dated 20/11/2020); A755-HTA-EV-A-DR-21710 (Revision P6; dated 20/11/2020); A755-HTA-EV-A-DR-21711 (Revision P6; dated 20/11/2020); A755-HTA-EV-A-DR-21712 (Revision P3; dated 20/11/2020); A755-HTA-EV-A-DR-21713 (Revision P3; dated 20/11/2020); A756-HTA-EV-A-DR-21810 (Revision P6; dated 20/11/2020); A756-HTA-EV-A-DR-21811 (Revision P6; dated 20/11/2020); A726-HTA-SC-DR-30000 (Revision P6; dated 20/11/2020); A726-HTA-SC-DR-30001 (Revision P3; dated 20/11/2020); A726-HTA-SW-A-DR-18000 (Revision P2; dated 15/12/2020); A726-HTA-SW-A-DR-18001 (Revision P2; dated 15/12/2020); A726-HTA-SW-A-DR-18002 (Revision P3; dated 15/12/2020); A726-HTA-C0-PS-A-DR-91000 (Revision P7; dated 15/12/2020); A726-HTA-C0-PS-A-DR-91001 (Revision P9; dated 15/12/2020); A726-HTA-C0-PS-A-DR-91002 (Revision P5; dated 15/12/2020); A726-HTA-C0-PS-A-DR-91003 (Revision P4; dated 15/12/2020); A726-HTA-C0-PS-A-DR-91004 (Revision P3; dated 15/12/2020); A726-HTA-C0-PS-A-DR-91005 (Revision P2; dated 15/12/2020); A726-HTA-C0-PS-A-DR-91006 (Revision P4; dated 15/12/2020); A726-HTA-C0-PS-A-DR-91007 (Revision P2; dated 15/12/2020); A726-HTA-C0-PS-A-DR-93004 (Revision P1; dated 20/11/2020); A726-HTA-C0-PS-A-DR-93005 (Revision P1; dated 20/11/2020); A726-TPP-G-PLN-00001 (Revision P2; dated 15/12/2020); A726-HOD-G-REP-00002 (Revision P1; dated 20/11/2020); A726-HOD-G-REP-00003 (Revision P1; dated 20/11/2020); A726-FDS-H-REP-00001 (Revision P1; dated 20/11/2020); A726-FDS-H-REP-00002 (Revision P5; dated 20/11/2020); A726-DEL-T-REP-00001 (Revision P2; dated 15/12/2020); A726-TPP-G-PLN-00002 (Revision P2; dated 15/12/2020); A726-BSO-G-REP-00001 (Revision P1; dated 20/11/2020); A726-HOD-G-REP-00001 (Revision P1; dated 20/11/2020); A726-TPP-G-REP-00001 (Revision P2; dated 15/12/2020); A726-HAN-A-REP-00001 (Revision P1; dated 20/11/2020); A726-RWD-H-REP-00002 (Revision P1; dated 20/11/2020).

Reason: For the avoidance of doubt and in the interests of proper planning.

### Materials

3. Samples of all external materials shall be submitted to and approved in writing by the local planning authority prior to the commencement of works on site (except for site clearance, demolition and enabling works). The development shall be constructed in accordance with the approved materials and be retained as such, thereafter.

Reason: To ensure that the materials harmonise with the surroundings in accordance with policies 1.1(h) and 2.1(c) of the Ealing Development Strategy (2012), policies D3 and D4 of the London Plan (2021) and policy 7B of the Ealing Development Management Development Plan Document 2013.

### Details required (refuse, cycle and car parking)

4. Notwithstanding the plans hereby approved no work shall commence (except for site clearance, demolition and enabling works) until further details of refuse collection points, secure cycle parking storage, car parking space layout within the podium areas as they relate to safety zones, junction design and perpendicular bays shall be submitted in writing to and approved by the local planning authority, Ealing Council. Such details shall thereafter be implemented in accordance with those approved details and be thereby retained.

Reason: To secure an inclusive, safe and high-quality built environment for all users of the site and be sustainable, and where appropriate constrain local highway impact. In accordance with policies T5, T6, T6.1 and S18 of the London Plan (2021), policies 1.1(e), 1.1(f), 1.1(g) and 1.1(h) of Ealing's adopted Development (or Core) Strategy 2026 (2012); Policies 6.13 and 7.3 of Ealing's Development Management DPD, 2013.

Landscaping

5. The details of hard and soft landscaping including the tree protection and tree planting proposals as submitted to and hereby approved by the local planning authority shall be laid out and planted prior to the occupation of the development. For the avoidance of doubt, details shall include details of the proposed soil, depth, tree pits and their short- and long-term irrigation. Any trees or plants which within 5 years of planting, die, are removed or become seriously damaged or diseased shall be replaced with others of the same size and species and in the same positions within the next planting season. The landscaping shall thereafter be maintained permanently.

Reason: To ensure that the development is landscaped in the interests of the visual character and appearance of the area in accordance with policies G4, G5 and G6 of the London Plan (2021).

6. Details of all boundary walls and/or fences shall be submitted to and approved in writing by the Local Planning Authority and in place prior to the first occupation of the development hereby approved/commencement of the use and permanently retained thereafter.

Reason: To ensure a satisfactory and safe development, in accordance with policies 1.1 (e) (g), 1.2(f), 2.1(c) of the Ealing Development (Core) Strategy (2012), policies ELV 7.4 and 7B of the Ealing Development Management Development Plan Document (2013) and policy D4 of the London Plan (2021).

7. Prior to the final occupation, a 5-year landscape management and maintenance plan, which shall include details of the long term design objectives, management responsibilities, and maintenance schedules for all landscape areas, shall be submitted to and approved in writing by the Local Planning Authority, on a full development or phased basis as appropriate. The approved landscape management and maintenance plan shall be implemented from the completion date of approved development and retained thereafter.

Reason: To ensure that the development is landscaped in the interests of the visual character and appearance of the area in accordance with policies G4, G5 and G6 of the London Plan (2021).

8. Prior to the commencement of the final delivered phase, details of permanent playspace equipment shall be submitted and approved in writing by the Local Planning Authority. The approved playspace equipment shall be maintained thereafter on an interim or permanent basis as appropriate.

Reason: To ensure that the development is in accordance with policy S4 of the London Plan (2021).

9. Notwithstanding the plans hereby approved, details of the green roof construction and specifications shall be submitted to and approved in writing by the Local Planning Authority prior to the commencement of the superstructure. The development shall thereafter be carried out in accordance with the approved plans and details. Details to include:
  - a) Substrate depth;
  - b) Layout of the roof;
  - c) Species mix;
  - d) Drainage system.

The green roofs shall not be used as amenity or sitting out space of any kind and shall be maintained as such thereafter and no change therefrom shall take place without the prior written consent of the Local Planning Authority.

Reason: To ensure that the development is landscaped in the interests of the visual character and appearance of the area in accordance with policies G4, G5 and G6 of the London Plan (2021).

Low NOx Boiler specification

10. Notwithstanding the plans hereby approved, details shall be submitted to the Local Planning Authority for approval before Phase 3 of the development is commenced, (except for site clearance, demolition and podium) for the installation of Ultra Low NOx communal gas boilers with maximum NOx emissions of under 0.040 g/kWh. The works, as approved, shall be completed prior to occupation and thereafter permanently retained.

Reason: To minimise the impact of building emissions on local air quality in the interests of health, in accordance with policy S11 of the London Plan (2021), the Mayor’s Sustainable Design and Construction SPG; policies 1.1(e) and (j) of Ealing’s Development (or Core) Strategy 2012, and policy 7A of Ealing’s Development Management DPD.

Details required Sound Insulation

11. Notwithstanding the plans hereby approved further details of the flat layouts shall be submitted to and approved in writing by the local planning authority prior to the completion of the super structure of the first block to be constructed. Such details shall demonstrate an enhanced sound insulation value  $D_{nT,w}$  and  $L'_{nT,w}$  of at least 5dB above the Building regulations value, for the floor/ceiling/wall structures separating different types of rooms/uses in adjoining dwellings and communal uses, for example, living/dining room and kitchen above/below/adjoining bedroom of separate dwelling/bedrooms adjoining communal rooms/stairs/lifts etc. The approved details shall be fully implemented prior to first occupation of any of the residential dwellings and be permanently retained thereafter.

Reason: To safeguard the amenities of the occupiers of the residential accommodation within the development.

**Informatives:**

1. The decision to grant planning permission has been taken having regard to the policies and proposals in the Ealing Development (Core) Strategy 2012, the Ealing Development Management Development Plan Document 2013, the London Plan 2016, the London Plan (2021), the National Planning Policy Framework 2018 and to all relevant material considerations including Supplementary Planning Guidance:

National Planning Policy Framework (2018)

1. Introduction
2. Achieving sustainable development
3. Plan-making
4. Decision-making
5. Delivering a sufficient supply of homes
6. Building a strong, competitive economy
7. Ensuring the vitality of town centres
8. Promoting healthy and safe communities
9. Promoting sustainable transport
11. Making effective use of land
12. Achieving well-designed places
14. Meeting the challenge of climate change, flooding and coastal change;
15. Conserving and enhancing the natural environment

16. Conserving and enhancing the historic environment

Annex 1: Implementation

Annex 2: Glossary

London Plan (2021)

- GG1 – Building strong and inclusive communities
- GG2 – Making the best use of land
- GG3 – Creating a healthy city
- GG4 – Delivering the homes Londoners need
- GG5 – Growing a good economy
- GG6 – Increasing efficiency and resilience
- SD1 – Opportunity Areas
- SD6 – Town centres and high streets
- SD10 – Strategic and local regeneration
- D1 – London’s form, character and capacity for growth
- D2 – Infrastructure requirements for sustainable densities
- D3 – Optimising site capacity through the design-led approach
- D4 – Delivering good design
- D5 – Inclusive design
- D6 – Housing quality and standards
- D7 – Accessible housing
- D8 – Public Realm
- D9 – Tall buildings
- D10 – Basement development
- D11 – Safety, security and resilience to emergency
- D12 – Fire safety
- D13 – Agent of change
- D14 – Noise
- H1 – Increasing Housing Supply
- H5 – Threshold Approach to Applications
- H6 – Affordable Housing Tenure
- H7 – Monitoring of affordable housing
- H10 – Housing Size Mix
- S4 – Play and Informal Recreation
- G1 – Green Infrastructure
- G4 – Local green and open space
- G5 – Urban greening
- G6 – Biodiversity and access to nature
- G7 – Trees and woodlands
- G9 - Geodiversity
- S11 – Improving air quality
- S12 – Minimising greenhouse gas emissions
- S13 – Energy infrastructure
- S14 – Managing heat risk
- S15 – Water infrastructure
- S16 - Digital connectivity infrastructure
- S17 – Reducing waste and supporting the circular economy
- S112 – Flood Risk Management
- S113 – Sustainable drainage
- S114 – Waterways - Strategic Role
- S116 – Waterways, Use and Enjoyment
- S117 – Protecting and enhancing London’s waterways
- T1 – Strategic approach to transport
- T2 – Healthy Streets
- T3 – Transport capacity, connectivity and safeguarding

T4 – Assessing and mitigating transport impacts  
T5 – Cycling  
T6 – Car parking  
T6.1 – Residential parking  
T6.5 – Non-residential disabled persons parking  
T7 – Deliveries, servicing and construction  
T9 – Funding transport infrastructure through planning  
DF1 Delivery of the Plan and Planning Obligations  
M1 – Monitoring

Mayor of London Supplementary Planning Guidance

Housing (March 2016)  
Use of planning obligations in the funding of Crossrail and the Mayoral CIL (April 2013)  
Social Infrastructure (May 2015)  
The Control of Dust and Emissions during Construction and Demolition (July 2014)  
Accessible London: Achieving an Inclusive Environment (October 2014)  
Affordable Housing and Viability (August 2017)

Ealing Development (Core) Strategy 2026 Development Plan Document, 2012

Chapter 1 - Vision for Ealing 2026  
1.1 Spatial Vision for Ealing (a), (b), (c), (d), (e), (f), (g), (h), (i), (j), (k)  
1.2 Delivery of the Vision for Ealing 2026 (a), (c), (d), (e), (f), (h), (i), (k), (m)  
Chapter 2 - Development in the Uxbridge Road/ Crossrail Corridor  
2.1 Realising the Potential of the Uxbridge Road/Crossrail Corridor (a), (b), (c), (d)  
2.8 Revitalise Southall Town Centre  
2.10 Residential Neighbourhoods  
Chapter 5 – Protecting and enhancing Ealing’s Green and Open Spaces  
5.3 Protect & Enhance Green Corridors (a)  
5.4 Protect the Natural Environment – Biodiversity and Geodiversity (a), (c)  
5.5 Promoting Parks, Local Green Space and Addressing Deficiency (b)  
5.6 Outdoor Sports and Active Recreation  
Chapter 6 – Ensuring Sustainable Delivery  
6.1 Physical Infrastructure  
6.2 Social Infrastructure  
6.3 Green Infrastructure  
6.4 Planning Obligations and Legal Agreements

Development Management Development Plan Document, 2013

Ealing Local Variation to London Plan Policy 2.18 Green Infrastructure: The Network of Open and Green Spaces  
Ealing Local Variation to London Plan Policy 3.4 Optimising Housing Potential  
Ealing Local Variation to London Plan Policy 3.5 Quality and Design of Housing Developments  
Ealing Local Policy - 3A Affordable Housing  
Ealing Local Policy 4A – Employment Uses  
Ealing Local Variation to London Plan Policy 4.5 London’s Visitor Infrastructure  
Ealing Local Policy 4B Retail  
Ealing Local Variation to London Plan Policy 5.2 Minimising Carbon Dioxide Emissions  
Ealing Local Variation to London Plan Policy 5.10 Urban Greening  
Ealing Local Variation to London Plan Policy 5.12 Flood Risk Management  
Ealing Local Variation to London Plan Policy 5.21 Contaminated Land  
Ealing Local Variation to London Plan Policy 6.13 Parking  
Ealing Local Policy 7A Amenities

Ealing Local Variation to London Plan Policy 7.3 Designing Out Crime  
 Ealing Local Variation to London Plan Policy 7.4 Local Character  
 Ealing Local Policy 7B Design Amenity  
 Ealing Local Variation to London Plan Policy 7.7 Location and Design of Tall and Large Buildings  
 Ealing Local Policy - 7D Open Space  
 EA Ealing Local Policy Presumption in Favour of Sustainable Development

Development Sites Development Plan Document, 2013

SOU5 Southall West

Southall Opportunity Area Planning Framework SPD (July 2014)

Ealing Informal Planning Documents/Guidance

SPG 3: Air Quality

SPG 4: Refuse and Recycling

SPG 9: Trees

SPD9: Legal Agreements, Planning Obligations and Planning Gain

In reaching this decision, specific consideration was given to whether the subject site is satisfactory for the proposed development, given its location, the impact on the amenities of occupiers of neighbouring residential properties and the character and appearance of the local area. All other considerations include affordable housing, housing mix, commercial uses, extension of Southall town centre, internal and external design matters, air quality, noise, sustainability, transport, community infrastructure levy and Section 106 planning obligations. Consideration was also given to consultation comments and relevant policies. It is considered, given the location together with the nature of the proposed uses and form of development, that planning permission could reasonably be granted in this case. It is not considered that there are any other material considerations, which would warrant a refusal of the application.

2. Demolition and construction works, audible beyond the boundary of the site shall only be carried on between the hours of 0800 - 1800hrs Mondays to Fridays and 0800 - 1300hrs on Saturdays and at no other times, including Sundays and Bank Holidays. No bonfires shall be lit on site. BPM & mitigation measures can be found in the following guidance:
  - i. 'Guidance on the Assessment of dust from demolition and construction', IAQM, February 2014
  - ii. 'The control of dust and emissions from Construction and Demolition' Draft SPG, GLA, 2013
  - iii. BS 5228-1:2009 - Code of practice for noise & vibration control on construction & open sites-Part 1: Noise
3. Prior to the commencement of any site works, all sensitive properties surrounding the site boundary shall be notified in writing of the nature and duration of works to be undertaken, and the name and address of a responsible person, to whom an enquiry/complaint should be directed. A minimum written period of 1 month would be required.
4. Dust mitigation and control of exhaust emissions from construction vehicles should comply with the Mayor's (GLA and London Councils) 'Best Practice Guidance' to control dust and emissions from construction.
5. The developer will be liable for the cost of any repairs to damage to the footway directly resulting from the construction work. It is recommended that a footway/carriage way condition survey is carried out prior to the start of construction work, in conjunction with the Highways Section.
6. To assist applicants in a positive manner, the Local Planning Authority has produced policies and written guidance, and offers and encourages a comprehensive pre-application advice



service, all of which is available on the Council's website and outlined in a 24 hours automated telephone system. The scheme complied with policy and guidance. The Local Planning Authority delivered the decision proactively in accordance with requirements of the National Planning Policy Framework.

7. Ground Investigation:

a) Reference should be made at all stages to appropriate current guidance and codes of practice this would include:

- The report of the findings must include:
  - A timetable of works and site management procedures.
  - (i) a survey of the extent, scale and nature of contamination;
  - (ii) an assessment of the potential risks to:
    - human health,
    - property (existing or proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes,
    - adjoining land,
    - groundwater and surface waters,
    - ecological systems,
    - archaeological sites and ancient monuments
- Model Procedures for the Management of Land Contamination, CLR 11, Environment Agency, 2004
- Updated technical background to the CLEA model, Science Report: SC050021/SR3, Environment Agency, 2009
- LQM/CIEH Generic Assessment criteria for Human Health Risk Assessment (2nd Edition), 2009
- BS10175:2011 Investigation of potentially contaminated sites – Code of Practice
- Secondary Model Procedure for the Development of Appropriate Soil Sampling Strategies for Land Contamination; Environment Agency, 2001
- Verification of Remediation of Land Contamination', Report: SC030114/R1, Environment Agency, 2010
- National Planning Policy Framework (Paragraph 109, 120, 121);
- Guidance for the safe development of housing on land affected by contamination, NHBC & Environment Agency, 2008

b) Clear site maps should be included in the reports showing previous and future layouts of the site, potential sources of contamination, the locations of all sampling points, the pattern of contamination on site, and to illustrate the remediation strategy.

c) All raw data should be provided in a form that can be easily audited and assessed by the council.(e.g. trial pit logs and complete laboratory analysis reports)

d) on-site monitoring for ground gases with any relevant laboratory gas analysis;

*'Good practice on the testing and verification of protection systems for buildings against hazardous ground gases, (C735), CIRIA, August 2014*

e) Details as to reasoning, how conclusions were arrived at and an explanation of the decisions made must be included. (e.g. the reasons for the choice of sampling locations and depths).

8. Noise

a) SPG10 requires that acoustic measurements are carried out and that precise calculations are made for the building envelope insulation. In calculating the minimum sound reductions the following is required:

- A precise sound insulation calculation under the method given at BS EN12354-3: 2000, for the various building envelopes, using the worst case one hour data (octave band linear

noise spectra from 63 Hz - 4k Hz) by night and day, to arrive at the minimum sound reductions necessary to meet the SPG10 internal data.

- Approved laboratory sound insulation test certificates for the chosen windows, including frames and seals and also for ventilators, in accordance with BS EN ISO 140-3: 1995 & BS EN ISO 10140-2:2010, to verify the minimum sound reductions calculated.

Compliance with the internal and external criteria set at SPG10

9. Ground Water Discharge: With regard to surface water drainage it is the responsibility of a developer to make proper provision for drainage to ground, water courses or a suitable sewer. In respect of surface water, it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off-site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of groundwater. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0845 850 2777.
10. A ground water risk management permit from Thames Water will be required for discharging ground water into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing [www.riskmanagement@thameswater.co.uk](mailto:www.riskmanagement@thameswater.co.uk). Application forms should be completed on line via [www.thameswater.co.uk/wastewaterquality](http://www.thameswater.co.uk/wastewaterquality).
11. Legal changes under the Water Industry (Scheme for the adoption of private sewers) Regulations 2011 mean that the sections of pipes you share with neighbours or are situated outside of your property boundary which connect to a public sewer are likely to have transferred to Thames Water's ownership. Should your proposed building work fall within 3 metres of these pipes we recommend that you contact Thames Water to discuss their status in more detail and to determine if a building over/near to agreement is required. You can contact Thames Water on 0800 009 3921 or for more information please visit the Thames Water website at [www.thameswater.co.uk](http://www.thameswater.co.uk)
12. The developer is advised that should any external plant be installed the rating noise level emitted from the proposed external plant and machinery at the proposed development, as assessed under BS4142: 1997, shall be lower than the existing background noise level by at least 5 dBA as measured at 3.5 m from the nearest ground floor sensitive facade and 1m from upper floor noise sensitive facades, during the relevant periods of operation.
13. Under the terms of the Water Resources Act 1991, and the Thames Region Land Drainage Byelaws 1981, the prior written consent of the Environment Agency is required for any proposed works or structures, in, under, over or within 8 metres of the top of the bank of the Yeading Brook, designated a 'main river'.
14. Under the terms of the Land Drainage Act 1991, the prior written consent of the Environment Agency is required for proposed works or structures which will affect the flow of the, designated an 'ordinary watercourse'.
15. The Thames Region (which includes all the London Boroughs) has been identified as an area of 'serious water stress' under our published document 'identifying Areas of Water Stress'.
16. All landscaping areas (including private gardens) should be planted with drought tolerant plants, trees and grasses to minimise water requirements.

17. Roof water down pipes should be connected to the drainage system either directly, or by means of back inlet gullies provided with sealing plates and drainage from covered car parking floors should pass through an approved oil separator before connecting to the surface water system.
18. The development of this site is likely to damage archaeological remains. The applicant should therefore submit detailed proposals in the form of an archaeological project design. This design should be in accordance with the appropriate English Heritage guidelines.
19. Prior to commencement of development of construction and demolition works, details of noise/vibration and dust mitigation measures shall be submitted to the Environmental Health section for approval.
20. Any stone crushing plant proposed on site during the demolition process may require authorisation under the Environmental Protection Act (Prescribed Processes and Substances) Regulations 1991, SI 472. Contact the IBE Environmental Health Section on 0208 825 8111.
21. The applicant/developer should refer to the current "Code of Practice for Works affecting the Canal & River Trust" to ensure that any necessary consents are obtained. Please visit <https://canalrivertrust.org.uk/business-and-trade/undertaking-works-on-our-property-and-our-code-of-practice>.
22. The applicant is advised that surface water discharge to the waterway will require prior consent from the Canal & River Trust. Please contact Claire McLean, Area Planner - London from the Canal & River Trust on 0203 204 4409 or at [Claire.mclean@canalrivertrust.org.uk](mailto:Claire.mclean@canalrivertrust.org.uk)